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**Land Use Consent Application
prepared for**

THE YOUTH HUB TRUST

109 Salisbury Street, Christchurch

**February 2020 (updated May 2020 to incorporate further
information)**

Land Use Consent Application
prepared for

THE YOUTH HUB TRUST

109 Salisbury Street, Christchurch

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Form 9: Application for Resource Consent Under Section 88 of the Resource Management Act 1991

TO: The Christchurch City Council

We: The Youth Hub Trust ('the applicant'), applies for the Land Use Consent described below.

1. The activity to which the application relates ('the proposed activity') is as follows:

Land use resource consent is sought for the construction and operation of a community facility, inclusive of a café, sheltered housing and six residential units.

The proposed activities for which consent is sought will be undertaken in accordance with the details, information and plans that accompany and form part of the application, including the Assessment of Effects on the Environment ('AEE') attached.

2. The site at which the proposed activity is to occur is as follows:

109 Salisbury Street, Christchurch. The site is legally described as Lot 7 DP 536430 contained in Record of Title 889770 (refer to **Appendix 1** for Certificate of Title).

The natural and physical characteristics of the site and any adjacent uses that may be relevant to the consideration of the application is set out in further detail within the details, information and plans that accompany and form part of the application, including the attached Assessment of Effects on the Environment ('AEE').

3. The full name and address of each owner or occupier (other than the applicant) of the site to which the application relates are as follows:

Owners: Social Service Council of the Diocese of Christchurch.

PO Box 1032, Christchurch 8140

Occupiers: the site is vacant

4. There are no other activities that are part of the proposal to which this application relates.

5. Additional consents required in relation to this proposal:

Land use consent may be required under the NES for Assessing and Managing Contaminants in Soil to Protect Human Health. That resource consent will be applied for separately.

6. I attach an assessment of the proposed activity's effect on the environment that—

(a) includes the information required by clause 6 of Schedule 4 of the Resource Management Act 1991; and

(b) addresses the matters specified in clause 7 of Schedule 4 of the Resource Management Act 1991; and



- (c) includes such detail as corresponds with the scale and significance of the effects that the activity may have on the environment.
7. I attach an assessment of the proposed activity against the matters set out in Part 2 of the Resource Management Act 1991.
 8. I attach an assessment of the proposed activity against any relevant provisions of a document referred to in section 104(1) (b) of the Resource Management Act 1991, including the information required by clause 2(2) of Schedule 4 of that Act.
 9. I attach an assessment of the proposed activity against the resource management matters set out in the relevant planning documents.
 10. I attach all necessary further information required to be included in this application by the district plan, the regional plan, the Resource Management Act 1991, or any regulations made under that Act.

Kim Seaton, Principal Planner

DATED: 27 February 2020

(Signature of applicant or person authorised to sign on behalf)

Address for service:

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Assessment of Effects on the Environment (AEE)



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Introduction

1. This application seeks land use consent to construct a facility for residential and community activities that will be used as a Youth Hub. The facilities will be utilised by organisations to deliver support services including healthcare, employment, education, and housing to young people between the ages of 10 to 25. An ancillary café will be located on the premises, together with a roof top market garden.
2. Section 88 of the Resource Management Act 1991 ('the Act') sets out the particular requirements for persons making an application to a local authority for a resource consent. Section 88(2)(b) states that:

"an application must be made in the prescribed form and manner; and include, in accordance with Schedule 4 of the Act, an assessment of environmental effects in such detail as corresponds with the scale and significance of the effects that the activity may have on the environment".

3. The following assessment is made in accordance with these requirements.

The Site and Surrounding Environment

4. The application site is known as 109 Salisbury Street and has a total net site area of approximately 4,250m². The relevant Certificate of Title is attached as **Appendix 1**.
5. The application site is located within the Christchurch's Residential Central City Zone in the Central City Outer Zone. An aerial photograph of the site is shown in Figure 1 below.
6. The subject site was formerly the Christchurch Bowls Club and still contains the bowling greens and buildings, located at 109 Salisbury Street. The Bowls Club premises were purchased in 2017 by the Anglican Church's social service agency Anglican Care for the sole purpose of developing a youth hub on the site.
7. The site currently contains a car park with 42 formed and marked spaces, with mature landscaping along the southern road frontage with Salisbury Street and trees and timber-paling or corrugate iron fencing along internal boundaries and the eastern road boundary with Gracefield Avenue.
8. Under the Christchurch Bowls Club ownership, the subject site was used for recreational games of bowls, bowls tournaments and social events.
9. The surrounding area is typical of areas on the edge of the Central City, featuring residential development consistent with the Residential Central City zoning, interspersed with a number of non-residential land uses.
10. A variety of activities are located within the vicinity of the application site. To the south of Salisbury Street and the subject site the land use activity becomes increasingly commercial towards the city centre.
11. The Salvation Army which is located to the east of the subject site across Gracefield Avenue, and Sheppard and Rout Architects which is located to the south of the subject site across Salisbury Street are examples of such non-residential activity within a 200m radius



of the subject site. The adjoining properties to the west, north and the south east of the site accommodate residential activities.

12. A residential development containing three storied attached residential units has recently been approved and constructed along the subject site's western boundary within 362 Durham Street North.

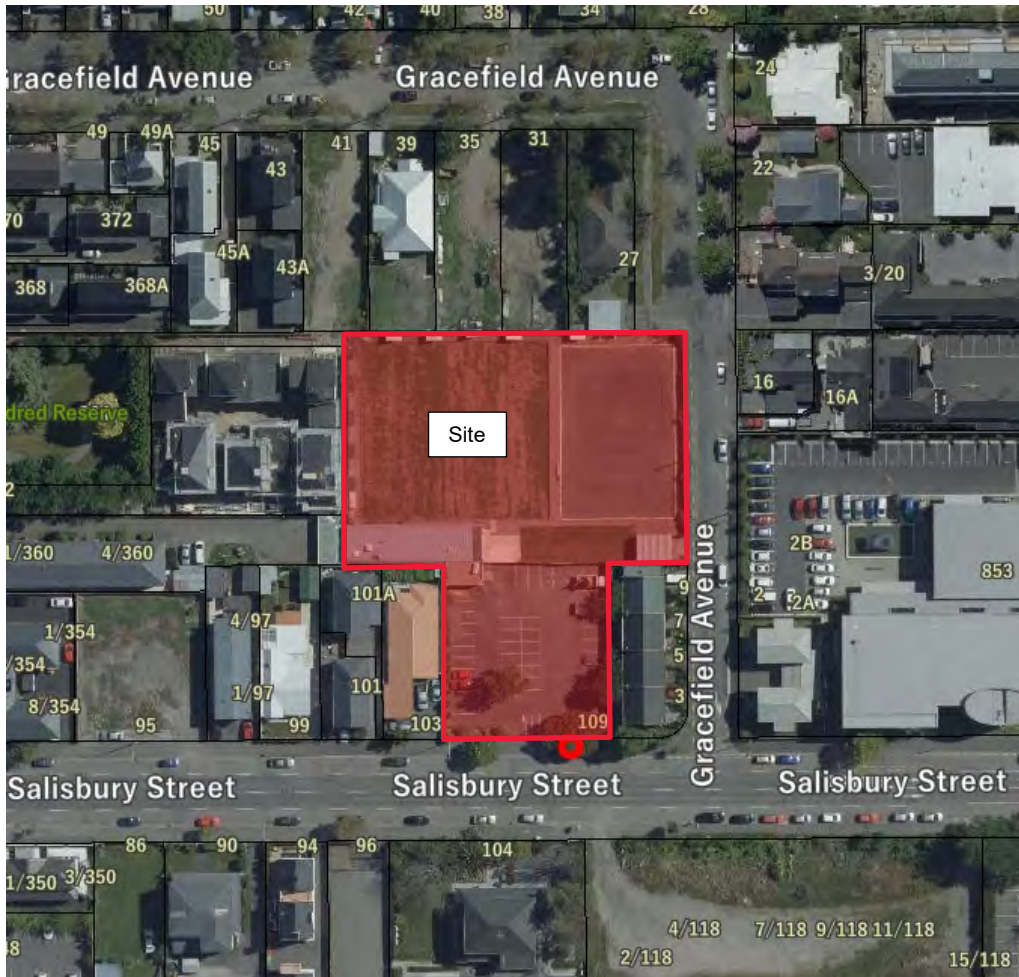


Figure 1: Site location (Source: Environment Canterbury)

13. The Salvation Army has an existing development to the east of the subject site across Gracefield Avenue, this includes church space, offices, residential units, kitchen facilities and associated parking.
14. Since the 2010 earthquakes the area has undergone considerable change with many sites near Gracefield Avenue having been cleared and awaiting development. This is a changing urban landscape. In terms of those buildings remaining, the landscape character of this area is derived from the mixture of building forms and scales that are expressive of a variety of land use activities including residential and commercial.
15. Salisbury Street is classified as a Central City Local Distributor road in the District Plan.



The Proposal

16. The application seeks consent for the construction of a community facility at 109 Salisbury Street, incorporating an ancillary café, together with sheltered living accommodation, six residential units and a market garden.

Built Form

17. The proposed facility will include:
- A maximum building height of approximately 9.94m, with varying building heights across the site;
 - Supervised accommodation for 22 youth (22 x 1 bedrooms) for persons aged 10 to 25, plus two supervisor's sleeping units within the western wing of the site;
 - 4 x three bedroom residential units and 1 x four bedroom residential unit, and one on site manager's residential unit within the eastern wing of the site;
 - Private and communal outdoor living space, predominantly in the form of ground floor courtyards but also including roof terraces;
 - Indoor recreation and art space;
 - An external basketball court;
 - A café inclusive of outdoor seating within a courtyard;
 - An events centre/hall that seats up to 200 persons;
 - Office space for government and non-government organisations that deliver health and welfare, employment, training/education, support, opportunities for creativity and recreation and housing services to youth;
 - A youth healthcare centre, for doctor and nurse visits, as well as counselling and youth worker visits;
 - A library (youth organisation's resource);
 - A rooftop greenhouse located centrally within the site. The greenhouse will be operated by Cultivate Christchurch, with an emphasis on vocational training, work and youth development. Some product from the greenhouse will be available for sale from the on-site café; and
 - Meeting/interview rooms and training space for staff and organisations operating from the site and their visitors.
18. Plans illustrating the proposal are contained in **Appendix 2**. Landscape plans are attached in **Appendix 3**, noting that comprehensive landscaping is proposed at both ground and upper floor levels, in addition to the roof top green house.



19. A two metre high acoustic fence will be installed on the internal boundary with all neighbouring properties.

Parking and Access

20. Vehicle access will be available to the site via Salisbury Street and Gracefield Avenue. The principal entrance will be from Salisbury Street, where four visitor parking spaces including one mobility space will be available. Six parking spaces including one mobility space will be available to staff, from Gracefield Avenue. All parking spaces will be marked for either visitor or staff use. The Salisbury Street car park will be accessible to vehicles during the hours of 7:00am to 11:00pm only.
21. 42 covered cycle parking spaces off Gracefield Avenue, 36 covered cycle spaces off Salisbury Street and 17 uncovered cycle spaces off Salisbury Street will be provided. Those spaces will also be suitable for other micro-transport options such as scooters.
22. In regards pedestrian access, as noted above it is intended that Salisbury Street will be the principal entrance to the site for visitors and staff. The Gracefield Avenue entrance will function as a secondary, more discrete entrance to the site for staff, residents and visitors and access via this route directly to the health care centre will be possible for persons (pedestrians) with previously arranged appointments. Residents of the eastern wing will be able to enter their flats directly from Gracefield Avenue.

Site Usage

23. The following organisations are proposed to be located within Youth Hub:
 - Youthline;
 - Supporting Families in mental illness;
 - Te Kura;
 - 298 Youth Health- Counselling and Youth Services;
 - Christchurch Methodist Mission;
 - Catapult;
 - Cultivate Christchurch;
 - Family Works;
 - The Collaborative Trust;
 - Qtopia;
 - Community Law;
 - City Mission; and



- Nurse Maude.
24. There will be different levels of presence within the site from the above organisations. Organisations such as Youthline and 298 Youth Health will have offices within the proposed development that will be utilised full time, while organisations such as Community Law will utilise rooms as needed.
 25. There is the potential for other organisations to join Youth Hub as the spaces proposed through this application have been designed to enable a wide variety of organisations to use the proposed facilities. All organisations operating from the site will be directly associated with the provision of support and services for youth.
 26. Table 1 below lists the predicted level of activity for all the uses of the proposed Youth Hub. The number of participants in each activity will vary from time to time.
 27. An operational statement by the Chair of The Youth Hub Trust is attached in **Appendix 4**. The Statement confirms how the management of the site will be undertaken. In respect of the residential components, two phases of support are proposed: the first phase being sheltered living within the western wing, with shared kitchen, lounge and laundry, and youth workers living on site to teach living skills and ensure behavioural standards of respect are maintained. Residents may reside in the sheltered living for between 3 and 18 months. The second phase, to which sheltered living residents may progress or transition¹, provides for independent residential units in the eastern wing, with no live-in support, but access to daily youth worker support. An on-site manager will reside in the eastern wing.
 28. In order to be eligible for housing, all young people will need to be in education/training or looking for employment. No alcohol or drugs will be permitted on the premises. The Christchurch Methodist Mission will oversee the management of the housing. Notably, rent subsidies will be available for youth residing on the site, ensuring that all residential accommodation on the site is affordable.
 29. The Youth Health Centre will be located centrally within the site. It has been operating as a one stop shop since 1995. The Health Centre will triage admissions to the housing on the basis of need and suitability.
 30. Entrance to the site for youth seeking to access services will principally be via the welcome/maanaki space at the Salisbury Street entrance. Visitors to the Salisbury Street entrance will be greeted by staff on entry and assisted or directed towards the services and support they require.
 31. As indicated above, there will be staff with specialist experience in behavioural management present on site during day time hours throughout the week. Outside normal business hours when residents only are present on site, three behavioural management staff will permanently reside on site.

¹ The eastern residential units will also be available to youth that have not transitioned through the sheltered living on site, at the discretion of management.



Table 1: Proposed Site Usage

Activity/Area	Use/Scale	Frequency	Staff	Participants/Visitors
Healthcare Services	298 Youth Health will be run full time from the Youth Hub with free medical care and counselling services.	Weekdays, 9:00am to 5:00pm.	11 part time and 5 full time (8-12 typically on site at one time)	Average 30 per day
Sheltered residential accommodation	Short-term accommodation (e.g. 3-18 months) is proposed to be provided onsite to accommodate up to 22 youth residents. There will be curfews and hours of operation which residents will be required to adhere to. Two supervisor's sleeping units are located in the same wing.	7 days, 24 hours	2	22
Independent Residential Accommodation	4 x three bedroom units and 1 x four bedroom unit	7 days, 24 hours	-	16 - 32
Managers	1 x on site manager's unit (1 x bedroom)	7 days, 24 hours	-	1-2
Café	A café is proposed that will provide work-based training. The café will be open to the general public. No alcohol will be served. No external music will be played in the café courtyard. A maximum of 10 seats in the courtyard. Produce sales	7 days, maximum hours 9:00am – 5:00pm	3-5	Seating for up to 35
Market Garden	Cultivate Urban Farm proposes to operate a market garden from the roof top green houses	Monday to Friday, 8:00am to 4:00pm	12	Up to 25 at one time
Recreation/Arts Centre	For use by residents and youth of the wider community.	Daily 9:00am to 9:30pm.	2-4	Up to 50 at one time but generally less
Events Centre	Events may be held on site not later than 10:00pm, on any day of the week. Events will all relate to youth development and support services on site (community activities)	Within the hours 09:00am to 10:00pm. Up to 12 times per year – up to 200 persons at one time up to 10:00pm. Friday or Saturday evenings only. Up to 12 times per year – 60 - 120 persons at one time, up to 10:00pm. Friday	2-4. Additional security staff possible for large events.	Up to 200



Activity/Area	Use/Scale	Frequency	Staff	Participants/Visitors
		or Saturday evenings only. Unlimited - <60 persons at one time, up to 9:30pm, 7 days.		
Office spaces	The office space will be used fulltime by organisations such as Youthline as well as part-time and occasionally by other organisations associated with the community facility.	7 days a week by Youthline from 7:00am to 6:00pm and Weekdays 09:00am to 5:00pm for other visiting agencies and persons. Hours and days are variable within those parameters depending on demand. Some agencies (e.g. Collaborative) will have Board meetings after 5:00pm once per month in the office meeting areas. Youthline call centre will operate 7 days from 8.00am to 12.00am.	Approximately 20 per day typically but up to 50. Variable. Within this number, Youthline call centre operate the following staff: 1 fulltime, and 4 part-time, plus 4 volunteers working in 3 hour shifts	Up to one visitor per office per hour, but highly variable.
Meeting spaces/training rooms	Meeting spaces will be used by staff, organisations, and visitors for interviews, training, education and other support functions for youth.	Typically 8.00am-5.00pm 7 days, with some evening training and board meetings etc. up to 10.00pm.	Occupancy of up to 40 people, with expected typical use of approximately 30 at any given time	
Welcome/Maanaki space	Drop in space and facility interface for visiting youth.	Typically 09:00am to 5:00pm weekdays. Also in use when events are on outside of these hours.	2 staff at any given time and variable numbers of volunteers (up to 3 at any given time). Extra staff (i.e. security) when required for events.	N/A (entry to wider uses/activities).

Earthworks

32. The majority of earthworks on the site will be associated with the establishment of building foundations and will not occur until building consent has been obtained. Those earthworks are therefore exempt from consideration under Rule 8.9.3(a)(iv). Earthworks required for



the exterior ground level areas are estimated to be approximately 284m³ in volume for excavation, with an equivalent amount of base course and hard surfacing replacement fill, to a depth of approximately 200mm. It is anticipated that approximately 220m³ of fill will be required, to a depth of around 400mm, to establish the greenhouses and roof top gardens across the site.

Statutory Context

NES for Assessing and Managing Contaminants in Soil to Protect Human Health

33. The NES controls soil disturbance on land where an activity on the Hazardous Activities and Industries List (HAIL) is being carried out, has been carried out, or is more likely than not to have been carried out.
34. The Listed Land Use Register ('LLUR') held by Environment Canterbury, indicates that the NES does apply given the former use of the site as a bowling club. Pre-application discussions with Council, and initial investigations, indicate that contamination levels are likely to be low. The City Council has advised that it will be acceptable to apply for resource consent under the NES separately, and subsequent to, obtaining land use consent for the proposed facility and residential use.

Christchurch District Plan

35. The application site is located within the Residential Central City Zone in the Christchurch District Plan. This zone typically entails a range of housing types, including attractive, high density living opportunities, and utilises the potential for living, working and playing in close proximity to the commercial centre of the city. The site is located within the Liquefaction Management Area, Central City Building Height 14m Overlay, Central City Outer Zone and Category 3: Lower Noise Level Area.
36. A compliance assessment is included in **Appendix 5** and based on that, resource consent is required for the matters set out in **Table 2** below. The compliance assessment in **Appendix 5** also sets out activities that are permitted on the site, including residential activity and market gardens. For reference, market gardens are defined by the District Plan as:

'in relation to residential zones, means a business growing a diverse range of fruit, vegetables and flowers as cash crops for general sale.'

37. Community facilities have a notably broad definition in the District Plan. The District Plan defines a community facility as follows:

'means any land and/or buildings used for community activities or education activities. Community facilities include reserves, recreation facilities, libraries, community infrastructure such as community halls, health care facilities, care facilities, emergency service facilities, community corrections facilities, community welfare facilities and facilities used for entertainment activities or spiritual activities.'



Community facilities exclude privately (as opposed to publicly) owned recreation facilities, entertainment activities and restaurants.'

38. Community activities are similarly broadly defined as follows:

'means the use of any land and/or buildings principally by members of the community for recreation, entertainment, health care, safety and welfare, spiritual, cultural or deliberation purposes.'

39. Residential activities are also broadly defined to include a range of residential activity and includes sheltered housing. The definition is as follows:

'means the use of land and/or buildings for the purpose of living accommodation. It includes:

a. a residential unit, boarding house, student hostel or a family flat (including accessory buildings);

b. emergency and refuge accommodation; and

c. sheltered housing; but

excludes:

d. guest accommodation;

e. the use of land and/or buildings for custodial and/or supervised living accommodation where the residents are detained on the site; and

f. accommodation associated with a fire station.'

40. It should be noted that residential activity may occur on a site but does not necessarily have to occur within a residential unit. Residential units are defined as below. The definition is relevant to which of the Youth Hub accommodation is located within a residential unit (the east wing) and which is not (the west wing):

'means a self-contained building or unit (or group of buildings, including accessory buildings) used for a residential activity by one or more persons who form a single household. For the purposes of this definition:

a. a building used for emergency or refuge accommodation shall be deemed to be used by a single household;

b. where there is more than one kitchen on a site (other than a kitchen within a family flat or a kitchenette provided as part of a bed and breakfast or farm stay) there shall be deemed to be more than one residential unit;

c. a residential unit may include no more than one family flat as part of that residential unit;

d. a residential unit may be used as a holiday home provided it does not involve the sale of alcohol, food or other goods; and

e. a residential unit may be used as a bed and breakfast or farm stay.'



Table 2: Non-compliances

General Rules and Procedures (Chapter 6)		
Rule	Requirement	Status
6.1.5.1.3 RD1	<p>Any activity listed in Rule 6.1.5.1.1 P1 or P3 that exceeds the noise limits in the activity specific standards by 10 dB or less.</p> <p>Comment – vehicles utilising the Salisbury Street car park will exceed the daytime LAeq noise limit specified in Rule 6.1.5.2.2 at 103 Salisbury Street by 4-8dB.</p>	Restricted Discretionary
Transport (Chapter 7)		
Standard	Requirement	Status
7.4.2.3 RD1	<p>a. Any activity that does not meet any one or more of the standards in Rule 7.4.3; or any activity that requires resource consent in accordance with Rule 7.4.3.10 - High trip generators except where otherwise provided for by Rule 7.4.2.2 C1.</p> <p>Comment – the proposal does not comply with the following standards:</p> <ul style="list-style-type: none"> - Rule 7.4.3.2 Minimum number of cycle parking facilities required (the layout of the cycle parks does not comply with the District Plan Requirements). - Rule 7.4.3.4 Manoeuvring for parking and loading areas (Additional manoeuvring is required to enter and exit parking spaces accessed via Gracefield Avenue). - Rule 7.4.3.7 Access design (The Gracefield Avenue access does not provide the required pedestrian visibility splay and is narrower than the required width (3.6m compared to 4.0m required)). - Rule 7.4.3.10 High trip generators (The proposed activity has been estimated as generating 34 to 83 vehicle movements per hour, so assessment is required against the Basic ITA matters). - Rule 7.4.3.11 Vehicle access to sites fronting more than one street – within the Central City (Access should only be taken from Salisbury Street, whereas access is also proposed to Gracefield Avenue). 	Restricted Discretionary
Subdivision, Development and Earthworks (Chapter 8)		
Rule	Requirement	Status
8.9.2.3 RD1	<p>Any activity listed in Rule 8.9.2.1 P1 or Rule 8.9.2.2 C1 that does not meet any one or more of the activity standards.</p> <p>Comment – the proposed earthworks do not meet rule 8.9.2.1 P1 as they will exceed the 20m³ of earthworks volume permitted within the site. The maximum depth of earthworks, at around 200-400mm, will comply.</p>	Restricted Discretionary
Residential (Chapter 14)		
Rule	Requirement	Status
14.6.1.3 RD1	<p>a. Any permitted activity or a cultural activity at 52 Rolleston Avenue Lot 2 DP496200), that does not meet one or more of the built form standards in Rule 14.6.2.</p> <p>b. Any application arising from the following built form standards shall not be limited or publicly notified:</p> <p>Rule 14.6.2.3 Road boundary building setback</p> <p>Rule 14.6.2.5 Fencing and screening</p>	Restricted Discretionary



- Rule 14.6.2.6 Tree and garden planting
- Rule 14.6.2.7 Minimum residential unit size
- Rule 14.6.2.8 Ground floor habitable space
- Rule 14.6.2.9 Outdoor living space
- Rule 14.6.2.10 Service space
- Rule 14.6.2.11 Minimum site density from development and redevelopment of residential units

Comment- *the proposal will not comply with Rule 14.6.2.5 Fencing and screening (a 2m high internal fence adjacent a car park will be established within 2m of the Salisbury Road frontage, noting a 1.8m high fence exists in that location currently), Rule 14.6.2.6 Tree and garden planting (less than 50% of landscaping will be in trees and shrubs), Rule 14.6.2.9 Outdoor living space (not all units will have private space directly accessible from their living areas and the private space will not achieve a minimum dimension of at least 4m), Rule 14.6.2.10 Service space (each of the 3 and 4 bedroom residential units will have 2.5m² outdoor service space rather than 3m²), Rule 14.6.2.11 Minimum site density (less than the 22 residential units required for minimum site density will be established on site).*

14.6.1.3 RD2	<p>a. Any activity involving the erection of new buildings and alterations or additions to existing buildings including all accessory buildings, fences and walls associated with that development, that result in:</p> <ul style="list-style-type: none"> i. three or more residential units; or ii. one or two residential units on a site smaller than 300m² gross site area; <p>b. Any application arising from this rule shall not be limited or publicly notified.</p> <p>Comment – <i>The proposal will result in six residential units.</i></p>	Restricted Discretionary
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14.6.1.5 NC1	<p>Any non-residential activity not otherwise provided for as a permitted, restricted discretionary, discretionary or non-complying activity with a gross floor area over 40m² (including any area of outdoor storage used for that activity).</p> <p>Comments- <i>the proposed site provides for a non-residential activity (café) with a gross floor area over 40m² that is not otherwise provided for in the residential central city chapter of the Plan. The proposal also provides for a community facility that is not otherwise provided for.</i></p>	Non-complying
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Activity Status - Summary

41. In summary, the proposal requires land use resource consent as a **non-complying activity** under the Christchurch District Plan.

Resource Management Act 1991- s95-95E and s104-104D

42. In terms of notification considerations in sections 95A-95E of the Act the following matters are noted:
 - i. public notification is requested by the applicant.
43. As a non-complying activity, the provisions in sections 104, 104B and 104D direct the substantive determination of applications and the following sections of this AEE have regard to the relevant provisions referred to therein.



Greater Christchurch Regeneration Act 2016

44. Section 60 of the Greater Christchurch Regeneration Act 2016 requires that:

Any person exercising powers or performing functions under the Resource Management Act 1991 must not make a decision or recommendation relating to all or part of greater Christchurch that is inconsistent with the Plan on any of the following matters under the Resource Management Act 1991:

(a) an application for a resource consent for a restricted discretionary, discretionary, or non-complying activity (whether or not the application was first lodged after the Plan was gazetted)

45. In determining the application, Council must be satisfied that the application is not inconsistent with any recovery plans relevant to the application.

Assessment of Actual or Potential Effects on the Environment

Assessment of Actual or Potential Effects on the Environment

46. Council's discretion to grant or decline the consent, or impose conditions is unrestricted and all actual and potential effects on the environment may be considered. However, the relevant effects requiring assessment are considered to relate to:

- Positive effects;
- Urban design;
- Noise effects;
- Transport effects;
- Earthworks effects;
- Effects arising from reduced site density;
- Residential amenity effects (internal); and
- Residential amenity effects (neighbours).

Positive Effects

A global leader benefiting the Christchurch community

47. Significant benefits for Christchurch youth, and by extension for the wider community, will accrue from the proposed development. The proposal will allow for multiple community organisations with a youth-focus to collaborate and share facilities from a single location, providing care, education, stability and services for the City's youth. The Operational Statement contained in **Appendix 4** sets out the value of the proposal to youth in the Christchurch community, noting that bringing youth focused organisations together on a single premise will allow them to work more effectively to provide solutions to the problems



that developing children and young people face. The Operational Statement also notes that often services that provide support are spread out in different parts of the city, meaning that they are inaccessible to a person with little money, no transport and no knowledge of who to go to or how to get there. The Youth Hub proposal brings together providers of health care, housing, catch up education, employment, training opportunities and other supports for easy access for young people who would otherwise go without.

48. The Operational Statement states that the project is believed to be at the forefront globally of integrated care and development for young people. Whilst the Operational Statement notes examples of integrated community facilities can be found, and examples of the residential housing model can be found, examples of both the community facility/services and residential housing for youth being fully integrated are understood to be rare.
49. More generally, the Operational Statement notes other positive benefits as including access by the wider community to music and drama performances by young people on site, and organic produce from the roof top greenhouses (Cultivate) that will be available for sale out of the on-site café.

Benefits for the residents from locating in a residential zone

50. The location of the proposed development within a residential zone is crucially important to the success of the project. Obvious benefits include a central location that it is readily accessible to all of the city's youth by public transport and is within walking distance of the city centre with its facilities such as Turanga, the Avon/Otakaro, the town hall, the Metro Sports Facility and the Margaret Mahy playground.
51. Less obvious but equally or more important benefits of the residential zone location relate to the Youth Hub's aspiration to assist young people to be citizens of the future. The Operational Statement explains that fostering community connection or a sense of belonging, being accepted by and being part of a community is vitally important to youth development, and that the importance of youth being part of a residential community cannot be overemphasised. That sense of community and connection, and of being a valued and responsible part of a community, cannot be achieved in a commercial or industrial area, where youth are literally separated from the City's residential communities.
52. It follows, that as important as it is for the youth to be resident in a residential community, the success of the development is equally dependent on the provision of the wrap around support services on the same site.
53. In terms of the benefits of the particular site location, Dr Sue Bagshaw, Chairperson of the Youth Hub Trust, has further stated (separately to the Operational Statement) that the site location is in a very pleasant residential neighbourhood, with plenty of greenery and a high amenity environment, and that it is a great area to live. Providing young people with a high quality home, with high quality support, in a high quality location is a strategic decision on the part of the Youth Hub Trust. Dr Bagshaw has noted that in order for young people not to feel marginalized or stigmatized, it is important to avoid putting them somewhere out of the way or in a neglected area or one full of abandoned buildings, bars or negative influences. The proposed site has the benefits of a sense of a relatively suburban setting, whilst also being central. The site also fulfills the desire expressed in feedback from youth in Christchurch, and is reinforced by research, for quiet and sanctuary, and connection with nature and other people.



Benefits for the residential neighbourhood

54. A further positive effect will be the introduction of residential activity to the site, by way of both sheltered accommodation and residential units, where currently the site is fully non-residential. All residential accommodation on the site will be subject to code of conduct requirements administered by the accommodation managers, and will have a level of oversight (on-site supervisors and managers) far exceeding that of “normal” residential development. The potential for nuisance issues or disruption to arise from the residential activity on the site is therefore considerably less than could occur from a more typical residential complex.

Urban Design

55. Urban design considerations are a relevant matter for assessment both for the proposed residential units (Rule 14.6.1.3 RD2), but also for the form and design of the complex as a whole.
56. The proposal was considered by the Christchurch City Council Urban Design Panel on 3 September 2019. The Panel recommendations are contained in **Appendix 6**. The Panel stated that it *‘...supports a use such as this in this location due to its centrality, access to public transport and connectivity to local services and other key amenities within the city. We see the benefit of bringing together such a wide range of services in one location for the youth of Christchurch. We commend the applicant for demonstrating their intention to be a good neighbour and looking to integrate the proposal into its residential context in a sympathetic manner.’* The Panel went on to make a number of recommendations, that were addressed as follows:

Key design and consent recommendations

- *consolidate the proposed café, gallery and on-site produce shop in order to maximise the opportunity for activity on Salisbury Street for the benefit of the wider neighbourhood* – the proposal has been amended to consolidate the produce store and café into a single facility (the café). The gallery is located within the main central building, near the café.
- *the setback to the boundary be increased and mitigated through a landscape component to reduce possible visual impact on its northern neighbours* – additional landscaping and substantial planting (trees) has been introduced on the northern boundary to mitigate the building’s position.
- *Supports basketball within this facility, however see a conflict between the basketball activities at rooftop level with the surrounding residential context, particularly with regards to noise and visual impact being out of context* – the rooftop basketball court proposal was abandoned. A ground level court is now proposed.
- *Supports the proposal for the productive planting, for its training opportunities and positive contribution to the environment. We encourage the continued development of the proposal, including fruit trees for variation in landscape scale and in lieu of additional car parking* – the productive planting proposal has been retained and developed in the form of the roof top greenhouse, a key aspect of the design proposal.



- *Recommends seeking traffic engineering advice to support the reduction in car parking numbers and increasing the provision of secured storage for micro-transport such as (e) bikes and scooters – traffic engineering advice is contained in the Transport Assessment in Appendix 7. Provision for both secured and unsecured micro-transport parking has been significantly increased.*
- *Encourages reconsideration of the internal organisation of the access spine corridor and the potential to open up to the courtyards and internal spaces for improved legibility / wayfinding and variation of the journey through the facility – internal organisation has been substantially reinvestigated, with more definition of internal and cross circulation. More and better integrated courtyard spaces have been created.*
- *Encourages the development of sustainable design strategies including water management, planting, energy systems, construction materials and alternative transport – substantial numbers of cycle/micro-transport parking are now provided, and use of public transport will be encouraged. Sustainable design strategies are continually being reviewed and will be further developed as detailed design progresses. Water recycling and passive ventilation are anticipated, and an emphasis on natural light. The rooftop greenhouse is an innovative utilisation of roof space, solar access and proximity to markets for on site produce.*

Secondary recommendations

- *Questions the feasibility of the extensive green roofs and suggests considering a more modest approach and incorporating plants in a variety of ways – Substantial areas of green roof have been removed and replaced with the Cultivate greenhouses. Plantings in terraces and a selected few green roofs remain.*
 - *Notes colour of the buildings / roofs can be managed to ensure visible outer coherence with the surrounding context while offering vibrancy in internal or inward facing surfaces – Colour in the buildings and roofs has been further considered and amended. Colour remains an integral design feature internally, with judicious use of colour on the external roofs.*
 - *Commends the holistic nature of the development, the inclusion of Māori kaupapa, engagement with mana whenua organisation Matapopore and its resonance with Te Whare Tapa whā (Mason Durie), and we encourage the creative exploration of water as a spiritual, healing and cleansing element across the site – the project is committed to continuing to work with cultural partners to further develop the design meaningfully.*
 - *Encourages increased attention to the presence of the facility on the Salisbury Street frontage, highlighting the welcoming nature of the entry and the embedded notion of manaaki that sits at the heart of this facility – the Salisbury Street entry remains emphasised in the design. The revised design includes a reiteration of the Gracefield Avenue entry to emphasis entry to the site. Powhiri and manaaki have also been considered in the revised sequence of spaces and courtyards to enter into the Youth Hub, in particular entry to the sheltered living.*
57. On the basis of the Panel comments and design response to the Panel's recommendations, it is considered that the proposal is supported by the Urban Design Panel.
58. An assessment of the proposed development, with particular reference to the urban design assessment matters set out in Rule 14.15.33 of the District Plan, has also been prepared



by Anne Wilkins, Principal Urban Designer and Landscape Architect, Novo Group. That assessment is attached in **Appendix 8**.

59. Ms Wilkins concludes as follows:

'The proposal is in keeping in character with the modified built-up landscape in which it is proposed. The Youth Hub can be suitably absorbed into the landscape fabric given the mix of residential and commercial activities existing, and the fringe nature of the area, meaning the development will blend into this particular urban environment.

The proposal aligns with the aims and objectives of the statutory district framework, specifically the Residential Design Guide 14.15.33, meeting the requirements. Amenity, function and integration matters; including access, streetscape, built forms, safety and CPTED, have been considered and are acceptable.

It is evident that urban design matters have been considered and developed over the duration of the proposal development. Efforts to ensure appropriate interfaces, material choices and offsets have been included to align with considered urban design matters. These will be further augmented by the landscaping proposed.

Landscape planting, including the design for separated courtyards, private spaces, recreational use, extensive planting and green roofs, is supported. The proposal also proposes a number of specimen trees and the retention of existing trees on site. These are considered appropriate, and furthermore of positive beneficial environmental gain (green roof and cultivation type areas). This will also assist in blending the proposal into the landscape, both at a localised and wider scale.

The development proposal will be complementary to the surrounding area, providing activation to the adapting urban environment, as well as the positive and beneficial aspects of a Youth Hub.'

60. Ms Wilkins' assessment is accepted and adopted for the purposes of this report. Her assessment is reinforced by the Design Statement, prepared by project architects Field, attached in **Appendix 9**. The Design Statement sets out the four design drivers for the project, being:

- a. The project aims, principles and values co-designed by local youth and organisations;
- b. Working sensitively and appropriately within the project site and context;
- c. Programming the site with the right mix of activities; and
- d. Resolving the Youth Hub's internal circulation in relation to the wider circulation patterns of its urban context.

61. The Design Statement sets out how the proposed design has responded to those drivers. The Design Statement concludes:

The Youth Hub design works to enmesh these four drivers to cater to wide range of stakeholders but most importantly, young people with diverse realities. Having worked to



include these diverse voices in the development of the principles that sit behind the design we feel confident that the resultant Hub will be a special one for local youth. In addition, the design works to carefully compliment the character and function of the surrounding neighbourhood with a more active frontage to Salisbury Street and a more residential character to Gracefield Avenue. This project is a significant investment in the future of Ōtautahi, socially, culturally and architecturally and we hope it will foster a sense of belonging and pride for Ōtautahi youth and the wider community.

62. On the basis of Ms Wilkins' advice and the statement of the project architects, together with the aforementioned Urban Design Panel comment, it is considered that the potential effects of the proposal in respect of urban design will be generally positive.

Noise Effects

63. The activity on the site will have a range of noise sources including car, residential and visitor activity and shop/cafe activity.
64. The proposed activity has been assessed by Novo Group's Environmental Acoustic Engineering Consultant Luke Sadler (see **Appendix 10**). This assessment has focussed particularly on the activities that could generate noise in breach of the District Plan noise rules and where there are nearby residential neighbours. The assessment finds the proposal will generally comply with the noise standards of the District Plan, with the exception of noise arising from vehicles utilising the Salisbury Street car park, which will exceed the daytime LAeq noise limit specified in Rule 6.1.5.2.2 at 103 Salisbury Street by 4dB as a result of normal day to day vehicle movements, and 8dB during the later day time hours of 10:00pm-11:00pm² when events are held on site.
65. In regard the potential adverse effects of the identified breach, Mr Sadler states that the noise effects of the proposal are unlikely to be considered unreasonable, when comparing the estimated car park noise levels with the estimated existing daytime background level of noise. In comparison to the estimated existing daytime background noise levels, activity within the Salisbury Street car park during day time hours is anticipated to result in a 1.5dB increase in noise levels for normal day to day activity, which is considered to be a barely perceptible change. The same comparative exercise for activity in the Salisbury Street car park during the later day time hours, on evenings that events are held, is estimated to result in a 6dB increase in noise levels beyond existing background levels. A 6dB increase will be noticeable, however Mr Sadler confirms that the increase is unlikely to adversely affect any person due to the relatively infrequent occasions events will be held, together with the sound insulating building materials of adjoining dwelling at 103 Salisbury Street (e.g. brick veneer and double glazing)³.
66. More generally, Mr Sadler outlines a number of recommendations for mitigation measures to enable the project to achieve permitted noise standards, good practice and to minimise potential adverse effects on neighbouring properties. Those recommendations are:

² The site is located within a Category 3 noise precinct, as defined by the Christchurch District Plan. For the purposes of establishing noise limits, the District Plan defines day time hours, being the period during which higher noise limits are set, as being 07:00 to 23:00 (Rule 6.1.5.2.2).

³ Only medium to large events may be held on site up 10:00pm, with only 24 such events permitted per year (average 2 monthly). All smaller events must finish by 9:30pm, to ensure that the Salisbury car park will be vacated by 10:00pm/



- a. construction activities will be managed and controlled in accordance with NZS6803:1999;
 - b. an acoustic fence should be installed at a height of 2m and shall be maintained in good condition with no gaps and shall have a minimum surface mass of 8.0kg/m²;
 - c. concrete walls of events room to be a minimum of 150mm thick;
 - d. implementing an event management plan to make sure people do not congregate in courtyards during events or in the car parks after events (ensuring people leave promptly from the car parks);
 - e. standard screening for HVAC systems;
 - f. Salisbury Street car park shall be used during the day time period only (07:00am to 11:00pm). Access to the car park will be physically prevented during night time hours;
 - g. maintain a 4m setback from the eastern boundary with regards to the setup of the café courtyard seating area, no outdoor music in the café courtyard and internal music at background levels only; and
 - h. ensuring that staff arriving in the early morning (05:00 – 07:00) can only park in the three southern parking spaces of the Gracefield Avenue Car Park.
67. Mr Sadler's recommendations are accepted and are adopted as part of the application. Recommendations (a), (d) and (e) are further recommended to form conditions of resource consent. An acoustic fence (recommendation (b)) is to be established on all internal boundaries. The project architects have confirmed that the events room walls will be a minimum of 150mm thick concrete. A 4m no-seating setback is specified on the Landscape Plan (Sheet L-101, **Appendix 3**) for the café courtyard (recommendation (f)). The Travel Management Plan will ensure that on occasions where staff arrive on site before 07:00am, they will only park in one of the three southern car parking spaces in the Gracefield Avenue car park (recommendation (g)).
68. On the basis of Mr Sadler's advice, and subject to the implementation of Mr Sadler's recommended mitigation, the proposal is considered to have adverse noise effects that are less than minor.

Transport Effects

69. A transport assessment has been undertaken by Novo Group's Senior Transport Engineer Nick Fuller and is included in **Appendix 7**.
70. Mr Fuller states that the proposed activities will provide for ten car parking spaces, including two mobility spaces, in two parking areas, with a predicted traffic generation rate of 34-83 vehicles per hour during peak periods. Peak parking demand is predicted to be 49 to 71 vehicles. On that basis, Mr Fuller acknowledges that the site will not provide sufficient car parking to accommodate likely demand, but that the reduced car park provision is consistent with the majority of Central City activities, the District Plan standards for car



parking in the Central City, and will encourage travel by alternate travel modes. Mr Fuller recommends a Travel Management Plan to further encourage and facilitate the use of modes other than private car travel, to the extent practicable, and provides a draft framework for the Travel Management Plan in Appendix 4 of his report. That recommendation to require a Travel Management Plan to be prepared, consistent with the framework outlined in Appendix 4 of Mr Fuller's report, is accepted and it is recommended that a condition of consent to that effect be imposed on the resource consent. Overall, Mr Fuller concludes that the effects of car parking supply and demand at the application site are considered to be acceptable.

71. Regarding cycle parking, Mr Fuller notes that the proposed cycle parking provision will exceed the requirements of the District Plan, with provision in the Travel Management Plan to monitor use of the cycle parks and increase provision if demand requires it. Mr Fuller considers the non-compliant layout of the cycle parking to be acceptable.
72. Regarding access, Mr Fuller considers both the Gracefield and Salisbury Street vehicle accesses are anticipated to operate satisfactorily, and concludes that the site is considered to be accessible by a range of transport modes.
73. The Transport Assessment concludes that, subject to the development of a Travel Management Plan that is consistent with framework set out in the Transport Assessment, the transport effects of the proposed activity are considered to be less than minor and acceptable. Mr Fuller's advice is accepted and adopted.
74. Based on the above assessment, the transport effects of the proposal are concluded to be acceptable and less than minor.

Earthworks Effects

75. The subject site is located within the Liquefaction Management Area ('LMA'). The proposal is a permitted activity within a Liquefaction Management Area.
76. In regard to the earthworks proposed, beyond building foundations they will generally be limited to works necessary to establish hard surfaces within the site, with some minor additional work to establish garden beds, and fill necessary to establish roof top gardens. The depths of works required are not expected to exceed 200mm for hard surfaces and 400mm for the roof garden and in any case will be less than the 0.6m permitted by Rule 8.9.2.1 P1. The total volume of earthworks will however exceed the 20m³ of earthworks volume permitted within the site, as could reasonably be expected for development of a large site. Approximately 284m³ of excavation is anticipated at ground level with an equivalent amount of replacement base course material and hard surfacing as fill. Approximately 220m³ of soil/growing medium is anticipated to be required (fill) to establish the rooftop gardens.
77. The relevant matters for assessing earthworks effects broadly relate to nuisance, resources and assets, land stability, and amenity.
78. Potential impacts on sedimentation and dust nuisance will be controlled by an Erosion and Sediment Control Plan ('ESCP'), which will be prepared in accordance with Environment Canterbury's toolbox. It is anticipated that the requirement for an ESCP will be a condition of resource consent. In terms of any noise or disturbance effects during the filling process,



truck movements are not anticipated to be significant – given that they will be for a temporary period only and spread over a period of construction.

79. The existing soils have no noted productive value, and regardless the site is zoned for residential purposes, and not for any rurally productive purposes.
80. As the site and surround are flat, it is considered unlikely that there will be an impact in respect of stability of adjoining land.
81. In respect of visual amenity, much of the earthworks will not be visible from the street or surrounds (and in time they will be covered by hard surfaces), and where works are visible they will be temporary only.
82. All stormwater from impervious surfaces will be collected on-site, and these works will be confirmed as part of the Building Consent.
83. Based on the above assessment, it is considered that the earthworks associated with the proposal will have adverse effects that are less than minor.

Site Density

84. As a site that is being redeveloped with residential units, Rule 14.6.2.11 requires that a minimum density of 22 residential units is to be achieved. Although the site will provide living accommodation for at least 41 persons (potentially more, for any rooms that have double bed capacity such as within the eastern residential units), it will only have six units that meet the definition of a residential unit and so does not meet the required density. Rule 14.15.34 provides guidance as to relevant assessment matters for consideration of this non-compliance, and these are listed for consideration below.
 - a. *In considering the reduction in the number of residential units to be constructed on a site, the extent to which the opportunity for future development of the site, in accordance with the density standard, is maintained, and*
 - b. *The extent to which accommodating further residential unit(s) to meet the density standard would adversely affect amenity outcomes for occupants of the residential units and/or the adjacent properties, given the size of the site or its configuration.*
 - c. *Whether the minimum development intensification target of an average net density of 50 households per hectare within the Central City is being achieved; and*
 - d. *The extent to which residential activity in the Central City is restored and enhanced through a variety of housing types suitable for a range of individual housing needs, while providing for a progressive increase in residential population*
85. The site is intended to be fully developed, there will be no future opportunities for further residential development. It is not known whether the minimum development intensification target of 50 households per hectare is being achieved within the Central City. Whilst it would theoretically be possible to accommodate additional residential units on site, notably within the western wing, this would occur at the expense of the crucial sheltered living accommodation, or of the support services internally. It would not be possible to fit further residential units on the site in addition to the proposed development, without reducing the



extent of landscaping, outdoor living and recreational space on site, or increasing the building bulk on the residential interfaces. Loss of those spaces or increase in building bulk would be detrimental to both on site residents and adjoining residential areas.

86. The final clause, (d), is particularly relevant to this site. Although the proposal technically provides for only six units that meet the definition of residential unit, it nonetheless will be providing additional residential accommodation for 22 youth residents and 2 supervisors, within the west wing of the site. There is no equivalent housing available within central Christchurch City at present that meets the particular needs of youth for living facilities with extensive wrap around support facilities on site. The proposal will therefore provide an accommodation type that is for particular individual need, and that will increase the residential population of the area. In particular, it is noted that there has been no residential use of the site historically due to the presence of the Christchurch Bowls Club on the site for several decades.
87. Overall, although the site will technically not provide the number of residential units required, it will provide a large amount of residential accommodation, with the majority being in supervised and sheltered living arrangements. Notably, were the same bedroom numbers provided in a one and two bedroom residential unit format across the site, the proposal would achieve the density sought in the District Plan. As such, the potential adverse effects of the reduced density are considered to be less than minor.

Residential Amenity Effects

88. A key matter requiring assessment for this proposal is potential effects on residential character and amenity and the extent to which the proposed development might adversely affect the receiving environment as it exists, or the reasonably foreseeable future environment. Amenity effects may arise both internally (for future residents of the site) and externally.

Existing Amenity Values

89. The Resource Management Act 1991 defines amenity values as '*...those natural or physical qualities and characteristics of an area that contribute to people's appreciation of its pleasantness, aesthetic coherence, and cultural and recreational attributes*'. As described above in regard Site and Surrounding Environment, and in the Urban Design Assessment in **Appendix 8**, the qualities and character of the environment as it currently exists is mixed, with predominantly residential uses to the north and on adjoining eastern and western boundaries, with commercial uses evident to the south across Salisbury Street, the nearby Salvation Army building and mixed retail at Peterborough Street. The site is described in the Urban Design Assessment as being within a fringe area, where the commercial area merges with residential. The aesthetics of the area are similarly mixed, with a mix of single, two and three storey residential dwellings of generally medium density in the area, together with more obviously non-residential buildings. The former Bowls Club use of the site would have previously contributed a recreational and social (club rooms) aspect to the immediate neighbourhood amenity values.
90. In regard the Bowls Club, the site has historically been used for recreational activities and social gatherings within this area for some decades, and there is no established residential use of the site. The use of the site by the Bowls Club has ceased for some time and existing



use rights may no longer exist for the non-residential aspects of the site use. A review of resource consents held for the bowling use and social aspects indicate the site may have been operating on existing use rights to an extent, particularly for any social aspects. Resource consents are held for bowling club buildings on site on the southern boundary, including an office and store (RMA/1976/212), consent to erect a cool room on the clubs car park area (RMA/1985/634) and consent for a soil shed (RMA/1993/1663). Consent for temporary accommodation was issued 2013 (RMA/2013/1755), but that is not considered to be of relevance to this application given its temporary nature. Overall then, it can be confirmed that the site has historically been used for non-residential purposes, some non-residential buildings can be considered to form part of the existing environment by way of resource consents held (and still in existence), and while the site is currently unused, there is no residential use of the site currently.

91. How then might the proposal adversely affect the amenity values of a residential/commercial interface area with a history of non-residential on the site? This question is addressed below.

Internal Amenity

92. Turning first to the question of internal amenity, both positive and potentially adverse effects are anticipated to arise in respect of the proposal. Positive effects are largely those detailed above and relate to the provision of a secure and supported environment for residents, with significantly more social, educational and health support available on site than would ever normally be expected for residential development. The site is designed to act as a “village within a village” concept, with wrap around support, a pleasant environment with generous landscaping and both internal and external passive and active recreation opportunities.
93. Potential adverse effects on internal amenity potentially arise in respect of landscaping (less than 50% of landscaping is comprised of trees and shrubs), reduced private outdoor living space dimensions and availability directly to living areas for the residential units, and reduced outdoor service space availability for the residential units. In regard to the landscaping, it is considered that the provision of soft landscaping in excess of the 20% of site required by the District Plan will mitigate any potential adverse effects of reduced proportion of trees and shrubs, noting that the reduction is in any case not significant. The potential adverse effects of the landscaping non-compliance for internal residents is therefore considered to be negligible. In regard private outdoor living space for the eastern residential units, it is anticipated that residents will predominantly utilise the internal communal courtyard for outdoor living. The communal courtyard provides a significantly larger area of space for outdoor living for the residential units than required by the District Plan, providing a spacious area and opportunity for access to sunlight through the year. The communal outdoor living space will also be of a high quality, as indicated on the landscape plans contained in **Appendix 3**. It is considered that the proposed outdoor living space for the residential units is appropriate and will have less than minor adverse effects on future residents of the site.
94. In regard to the proposed outdoor service space of the residential units, the area available is 0.5m² less than required by the District Plan, however all units are considered to have sufficient and easily accessible space for the storage of bins outdoors, such that potential adverse effects will be minimal. All storage areas will be screened from sight of the street. The applicant notes that it is continuing to develop the finer details of the proposal and this



includes consideration of whether further storage spaces, either communal or internal, would be of functional benefit to the site.

External Amenity

95. Turning to potential effects on the amenity values of the surrounding external environment, notably, residential activity proposed on the site sleeves the north western and north eastern edges of the site, including for the full frontage of Gracefield Avenue. Non-residential activity, comprising the community facility aspects of the development and the café, are located centrally within the site and are well separated from Gracefield Avenue (e.g. 16 Gracefield Avenue) and the north western residents. While the visual bulk of the rooftop atrium greenhouse will be visible to those residents, it will be substantially distant and separate from those residential areas and in any case contains the market garden activity that is a permitted and therefore anticipated activity by the District Plan. Substantial planting is proposed on the periphery of the site, further enhancing the interface with residential areas. Potential adverse effects on existing amenity values of residents to the northwest and northeast are therefore considered to be less than minor.
96. In regard to residential neighbours to the north of the site, the residents at 35-41 Gracefield Avenue will immediately adjoin the sheltered housing area, either the accommodation itself or the associated living courtyard. 31 Gracefield Avenue will wholly adjoin the northern façade of the community facility building, where the health care and market garden aspects of the operation will be accommodated. 27 Gracefield Avenue will primarily adjoin the northern car park. The residents at 27 and 31 Gracefield Avenue will therefore not have the benefit of gaining an immediate residential neighbour, though equally they cannot be said to be losing a neighbour as no residential activity exists there currently. Potential adverse effects on residential coherency are therefore considered to be not worse than is currently the case. While the residents at 27 and 31 Gracefield Avenue, and to a lesser and reducing extent the residents of 35-41 Gracefield Avenue, will be adversely affected to some degree by the location of the central community facility building, the high quality of the design, compliance with built form standards for building height, setback and recession planes, and substantial planting on the northern interface, will ensure that potential adverse effects are less than minor. Notably, external non-residential activity at the northern end of the site will be limited, generally comprising only occasional movements of visitor, staff and resident cars in the Gracefield Avenue car park. The Acoustic Assessment (**Appendix 10**) has confirmed that the proposal will meet the noise standards of the District Plan at the northern extent of the site.
97. In regard to the residents at 3-9 Gracefield Avenue, those residents will adjoin the café and café courtyard, and a central courtyard. As with the residents at the northern end of the site, the residents of 3-9 Gracefield Avenue will not lose any existing residential neighbours. The unit at 9 Gracefield Avenue will gain one residential neighbour, being the 4 bedroom unit in the eastern wing of the proposed development. Potential nuisance noise effects arising from the café will be carefully managed, including by way of a no-seating seatback in the café courtyard and specifying that no music be played externally. Any music within the café must be played at background noise levels only. The managers of the café will ensure noise levels and behaviour associated with the café use is acceptable, noting that the café will be open for business during normal day time business hours only and will not be open in the evenings, when the potential for nuisance effects on neighbours will be



higher. An events management plan will be prepared, anticipated as a condition of consent, that will ensure that any use of the eastern central courtyard will be managed and supervised to avoid nuisance noise effects on the neighbours. It is noted that the residential units at 3-9 Gracefield Avenue are oriented primarily towards the Gracefield frontage, with predominantly small or high windows on the frontage with the application site, and that the interface between the two sites will be landscaped. Overall, it is considered that the potential adverse amenity effects on the residents at 3-9 Gracefield Avenue will be minor.

98. In regard to the residents at 101A Salisbury Street, that residence will gain residential neighbours, with a sheltered housing supervisors unit at ground level and a sheltered housing unit at first floor level. 101A Salisbury Street will otherwise be well separated from the community facility/non-residential components of the site. Adverse effects on the amenity values of persons at 101A Salisbury Street are considered to be less than minor.
99. The residents at 103 Salisbury Street will adjoin the Salisbury Street car park and internal pedestrian access within the site, as well as the centrally located community facilities including the arts centre and main site entrance. 103 Salisbury Street currently adjoins the Bowls Club car park, and for this reason vehicle movements and non-residential use of a neighbouring site will be familiar to the residents. It is noted that the Bowls Club car park has continued to be utilised for car parking purposes on weekdays at least, following the closure of the Bowls Club, though it is unclear whether this is legally established or not. Whilst the acoustic assessment supporting this application has concluded that potential adverse noise effects on the residents at 103 Salisbury Street will be acceptable, it is nevertheless acknowledged that the residents at 103 Salisbury Street will experience a level of non-residential activity adjoining it, beyond what it experiences currently. The large majority of non-residential activity will occur only within normal weekday hours, however there will be occasional use of the events centre and recreation centre during evening hours, resulting in non-resident persons accessing the car park and main entrance during the evenings. Whilst both the proposed events and traffic management plans will assist in ensuring effects on neighbours are minimised, the residents at 103 Salisbury Street will experience non-residential activity in periods when they currently do not. The orientation of the dwelling at 103 Salisbury Street will mitigate the additional activity to some extent, noting that its primary outdoor living space appears to be at the northern end of the site, away from the Youth Hub entrance and close to the sheltered living components of the site. Further, the hours of use of the events and recreation centres on the site have been limited, so that small events (<60 persons) must finish by 9:30pm so that the car park will be vacated by 10:00pm. Larger events (>60 persons) are restricted to a total of 24 per year, or an average of two per month, and may be held on Friday or Saturday evenings only, to reduce the potential for disturbance of the adjoining neighbours in the 10:00pm -11:00pm period. Those events must also be finished by 10:00pm, to ensure the car park is generally vacated by 10:30pm. Overall, the potential adverse effects on the amenity of residents at 103 Salisbury Street are considered to be minor, given the historic car park and non residential use of the site, the dwelling's location on a Central City Local Distributor road (i.e. an existing relatively busy road), the residential components proposed in the Youth Hub development, the limitations volunteered for the number of evenings per year that larger functions can be held on site and the orientation of the dwelling at 103 Salisbury Street.
100. In regard to properties on the southern side of Salisbury Street, it is noted that the site immediately adjacent the Youth Hub entrance (104 Salisbury Street) contains a non-



residential activity. Given the mitigating width of Salisbury Street and the non-residential use of the site, occupiers of the site are not considered to be sensitive to the non-residential aspects of the Youth Hub activity that front Salisbury Street, and effects on those parties are considered to be less than minor. Other existing or developing residential sites on the south side of Salisbury Street are considered to be sufficiently distant, that potential adverse effects on their amenity values will be less than minor.

101. Overall, it is considered that the proposed Youth Hub development will not significantly adversely affect the amenity values of the area, noting:

- The mixed residential / commercial amenity values present in the area currently, with the site located on the fringe of an increasingly commercial area;
- the residential activity sleeving the north western and north eastern boundaries of the site, positively contributing to the amenity of the environment by introducing residential activity to the site where currently there is none;
- the intention to support residents on the site to become members of the residential community (as set out in Positive Effects above);
- the landscaping proposed within the site to visually enhance the development and mitigate potential building bulk;
- the 24 hour on-site management and caretakers present on site;
- the high quality of architectural and urban design;
- the site will be open to health centre and Cultivate visitors only during weekday business hours;
- the café will be open only during normal day time business hours; and
- the site will be open for large or medium evening functions only occasionally (up to 24 times per year), ensuring that evening usage of the site by non-residents will generally be at low levels.

Summary of Actual and Potential Effects

102. In summary, the actual and potential effects of the proposal are concluded to be generally **less than minor**, except in so far as potential adverse effects on the amenity values of the residents at 3-9 Gracefield Avenue and 103 Salisbury Street are considered to be minor. Significant positive effects will arise from the proposal.



Relevant Provisions of Planning Instruments

103. The planning documents of relevance to this application and the provisions therein are listed and assessed in turn below:

Christchurch District Plan

104. The objectives and policies in the plan of relevance to this application are assessed below in Table 3:

Table 3: Assessment of relevant objectives and policies

District Plan provision	Comment / Assessment
Chapter 3- Strategic Directions	
<p>3.3.1 Objective - <i>Enabling recovery and facilitating the future enhancement of the district</i></p> <p><i>The expedited recovery and future enhancement of Christchurch as a dynamic, prosperous and internationally competitive city, in a manner that:</i></p> <p><i>a. Meets the community's immediate and longer term needs for housing, economic development, community facilities, infrastructure, transport, and social and cultural wellbeing; and</i></p> <p><i>b. Fosters investment certainty; and</i></p> <p><i>c. Sustains the important qualities and values of the natural environment.</i></p>	<p>The proposal supports Objective 3.3.1 through meeting the immediate and longer-term needs for housing, community facilities, and social and cultural wellbeing. As discussed in the assessment of positive effects above, the project is understood to be at the forefront internally of integrated care and development of young people.</p>
<p>3.3.4 Objective - <i>Housing capacity and choice</i></p> <p><i>a. For the period 2018-2048, a minimum of 55,950 additional dwellings are enabled through a combination of residential intensification, and greenfield development, made up of:</i></p> <p><i>i. 17,400 dwellings between 2018 and 2028, and</i></p> <p><i>ii. 38,550 dwellings between 2028 and 2048; and</i></p> <p><i>b. There is a range of housing opportunities available to meet the diverse and changing population and housing needs of Christchurch residents, including:</i></p> <p><i>i. a choice in housing types, densities and locations; and</i></p> <p><i>ii. affordable, community and social housing and papakāinga.</i></p>	<p>The proposal will provide 22 bedrooms for sheltered living accommodation (+ 2 supervisors' bedrooms) and 6 residential units, providing an opportunity for supported and affordable living space for young people. Housing opportunities such as this are in very limited supply in Christchurch at present. The site contains no residential development currently and the proposal therefore will provide for new intensification. The proposal is therefore supportive of this objective.</p>
<p>3.3.7 Objective - <i>Urban growth, form and design</i></p> <p><i>a. A well-integrated pattern of development and infrastructure, a consolidated urban form, and a high quality urban environment that:</i></p> <p><i>i. Is attractive to residents, business and visitors; and</i></p> <p><i>ii. Has its areas of special character and amenity value identified and their specifically recognised values appropriately managed; and</i></p> <p><i>iii. Provides for urban activities only:</i></p> <p><i>A. within the existing urban areas; and</i></p> <p><i>B. on greenfield land on the periphery of Christchurch's urban area identified in accordance with the Greenfield Priority Areas in the Canterbury Regional Policy Statement Chapter 6, Map A; and</i></p>	<p>As discussed in the assessment above, the proposal has received support from the Urban Design Panel and has been further confirmed as a high quality design by the statements in Appendices 6 and 8.</p> <p>The proposal is a re-development of an existing non-residential site in urban Christchurch. As the sheltered housing component does not meet the definition of a residential unit or units, the site will technically not achieve the District Plan density target for housing in the Central City area, however it will provide living accommodation for at least 41 persons</p>



- iv. *Increases the housing development opportunities in the urban area to meet the intensification targets specified in the Canterbury Regional Policy Statement, Chapter 6, Objective 6.2.2 (1); particularly:*
- A. *in and around the Central City, Key Activity Centres (as identified in the Canterbury Regional Policy Statement), larger neighbourhood centres, and nodes of core public transport routes; and*
 - B. *in those parts of Residential Greenfield Priority Areas identified in Canterbury Regional Policy Statement Chapter 6, Map A; and*
 - C. *in suitable brownfield areas; and*
- v. *Maintains and enhances the Central City, Key Activity Centres and Neighbourhood Centres as community focal points; and*
- vi. *Identifies opportunities for, and supports, the redevelopment of brownfield sites for residential, business or mixed use activities; and*
- vii. *Promotes the re-use and re-development of buildings and land; and*
- viii. *Improves overall accessibility and connectivity for people, transport (including opportunities for walking, cycling and public transport) and services; and*
- ix. *Promotes the safe, efficient and effective provision and use of infrastructure, including the optimisation of the use of existing infrastructure; and*
- x. *Co-ordinates the nature, timing and sequencing of new development with the funding, implementation and operation of necessary transport and other infrastructure.*

(the minimum likely, assuming only single occupancy of all rooms).

The facility supports the Central City as a community focal point, bringing young people into the Centre.

The facility has parking facilities for large numbers of cycle or micro-transport options and is well located relative to public transport networks and pedestrian access, given its proximity to the City Centre.

Overall, the proposal is considered to support this objective.

3.3.8 Objective - Revitalising the Central City

- a. *The Central City is revitalised as the primary community focal point for the people of Christchurch; and*
- b. *The amenity values, function and economic, social and cultural viability of the Central City are enhanced through private and public sector investment, and*
- c. *A range of housing opportunities are enabled to support at least 5,000 additional households in the Central City between 2012 and 2028.*
- d. *The Central City has a unique identity and sense of place, incorporating the following elements, which can contribute to a high amenity urban environment for residents, visitors and workers to enjoy:*
 - i. *a green edge and gateway to the City defined by the Frame and Hagley Park;*
 - ii. *a variety of public spaces including the Avon river, squares and precincts and civic facilities;*
 - iii. *built form and historic heritage that reflects the identity and values of Ngai Tahu, and the City's history as a European settlement; including cathedrals and associated buildings at 100 Cathedral Square and 136 Barbadoes Street;*
 - iv. *a wide diversity and concentration of activities that enhance its role as the primary focus of the City and region; and*
 - v. *a range of options for movement within and to destinations outside the Central City that are safe, flexible, and resilient and which supports the increased use of public transport, walking and cycling.*

As discussed in the assessment above and the Operational Statement in Appendix 4, a central location has been deliberately sought for the Youth Hub, both for ease of access, but also to bring young people into the city. The proposed development will introduce living accommodation to the Central City on a site where currently none exists, and will provide a focal point for the community. The proposal is supportive of this objective.

3.3.11 Objective - Community facilities and education activities

The proposal is for a community facility that will accommodate multiple youth focused community welfare and training agencies in a single shared facility, to meet the needs of the youth of



- a. *The expedited recovery and establishment of community facilities and education activities in existing and planned urban areas to meet the needs of the community; and*
- b. *The co-location and shared use of facilities between different groups is encouraged.*
- Christchurch. The proposal is very supportive of this objective.

3.3.14 Objective - Incompatible activities

- a. *The location of activities is controlled, primarily by zoning, to minimise conflicts between incompatible activities; and*
- b. *Conflicts between incompatible activities are avoided where there may be significant adverse effects on the health, safety and amenity of people and communities*

The Residential Central City zoning anticipates the residential accommodation and market garden aspects of the proposed Youth Hub. The community facilities and café will be managed through the presence of on-site staff to ensure that any nuisance or disruptive behaviour that might impact on adjoining residents is avoided. Site layout and building design has also been developed to mitigate potential adverse effects of non-residential activity on residential neighbours. The proposed activities will not significantly adversely affect the health, safety and amenity of the people and surrounding community. The proposal is not considered to be incompatible with the surrounding activities and the proposal is therefore consistent with this objective.

Chapter 6 General Rules and Procedures

6.1.2.1 Objective - Adverse noise effects

- a. *Adverse noise effects on the amenity values and health of people and communities are managed to levels consistent with the anticipated outcomes for the receiving environment.*

The Acoustic Assessment contained in Appendix 10 has confirmed that the proposed development will comply with the District Plan noise standards in all respects, with the exception of non-compliances arising from the use of the Salisbury Street car park that are considered to be acceptable in the context of existing background noise. A range of mitigation measures have been recommended and accepted to ensure noise effects are appropriately managed. No public access to the site for events or similar will occur during night time hours, and on-site resident staff will ensure that other residents of the site maintain quiet during night hours.

6.1.2.1.1 Policy - Managing noise effects

- a. *Manage adverse noise effects by:*
- i. *limitations on the sound level, location and duration of noisy activities;*
 - ii. *requiring sound insulation for sensitive activities or limiting their location relative to activities with elevated noise levels.*

Overall, the proposal is considered to be consistent with this objective and policies.

6.1.2.1.2 Policy - Noise during night hours

Achieve lower noise levels during night hours to protect sleep, and the amenity values of residential and other sensitive environments, so far as is practicable.

Chapter 7- Transport

7.2.1 Objective - Integrated transport system for Christchurch District

- a. *An integrated transport system for Christchurch District:*
- i. *that is safe for all transport modes;*
 - ii. *that is responsive to the current recovery needs, future needs, and enables economic development;*
 - iii. *that supports safe, healthy and liveable communities by maximising integration with land use;*
 - iv. *that reduces dependency on private motor vehicles and promotes the use of public and active transport;*
 - v. *that is managed using the one network approach.*

An Integrated Transport Assessment has been undertaken for the proposal and is contained in Appendix 7. That assessment has confirmed that the proposal will have acceptable transport related effects, and that the design of parking areas and access is satisfactory. The activity has been assessed as a high trip generating activity.

The proposal provides for limited on site carparking, with an expectation of large numbers of visitors and staff arriving by alternative transport modes, including

7.2.1.2 Policy - High trip generating activities



a. Manage the adverse effects of high trip generating activities, except for permitted activities within the Central City, on the transport system by assessing their location and design with regard to the extent that they:

- i. are permitted¹ by the zone in which they are located;*
- ii. are located in urban areas and generate additional vehicle trips beyond what is already established or consented, unless the already established or consented vehicle trips are specifically included in rule thresholds;*
- iii. are accessible by a range of transport modes and encourage public and active transport use;*
- iv. do not compromise the safe, efficient and effective use of the transport system;*
- v. provide patterns of development that optimise use of the existing transport system;*
- vi. maximise positive transport effects;*
- vii. avoid significant adverse transport effects of activities where they are not permitted by the zone in which they are located;*
- viii. mitigate other adverse transport effects, such as effects on communities, and the amenity values of the surrounding environment, including through travel demand management measures;*
- ix. provide for the transport needs of people whose mobility is restricted; and*
- x. integrate and coordinate with the transport system, including proposed transport infrastructure and service improvements.*

cycles and other forms of micro transport such as scooters. A central location has been deliberately chosen in part to enhance the accessibility of the site by modes other than car.

Overall, the proposal is considered to be consistent with this objective and policies.

7.2.1.3 Policy - Vehicle access and manoeuvring

a. Provide vehicle access and manoeuvring, including for emergency service vehicles, compatible with the road classification, which ensures safety, and the efficiency of the transport system.

7.2.1.4 Policy - Requirements for car parking and loading

a. Outside the Central City:

i. Require car parking spaces and loading spaces which provide for the expected needs of an activity in a way that manages adverse effects.

ii. Enable a reduction in the number of car parking spaces required in circumstances where it can be demonstrated that:

A. the function of the surrounding transport network and amenity of the surrounding environment will not be adversely affected; and/or

B. there is good accessibility by active and public transport and the activity is designed to encourage public and active transport use; and/or

C. the extent of the reduction is appropriate to the characteristics of the activity and its location; and/or

D. the extent of the reduction will maintain on-site parking to meet anticipated demand.

b. Within the Central City:

i. Enable activities to provide car parking spaces and loading spaces, whilst minimising any adverse effects on the efficiency and safety of the transportation networks, including public transport, to the extent practicable.

ii. Manage the development of commercial car parking buildings and parking lots within the Central City so that they:

- A. support the recovery of the Central City;*
-



- B. are easily accessible for businesses within the Central City;
- C. minimise any adverse effects on the efficiency and safety of the transportation networks of all users, to the extent practicable;
- D. protect the amenity values of the Central City;
- E. reduce the need for activities to provide their own on-site parking;
- F. do not significantly adversely affect the demand for public transport to, from or within the Central City.

iii. Allow for temporarily vacant sites to be used for car parking areas within the Central City until 30 April 2018.

7.2.1.5 Policy - Design of car parking areas and loading areas

a. Require that car parking areas and loading areas are designed to:

- i. operate safely and efficiently for all transport modes and users;
- ii. function and be formed in a way that is compatible with the character and amenity values of the surrounding environment; and
- iii. be accessible for people whose mobility is restricted.

7.2.1.6 Policy - Promote public transport and active transport

a. Promote public and active transport by:

- i. ensuring new, and upgrades to existing, road corridors provide sufficient space and facilities to promote safe walking, cycling and public transport, in accordance with the road classification where they contribute to the delivery of an integrated transport system;
- ii. ensuring activities provide an adequate amount of safe, secure, and convenient cycle parking and, outside the Central City, associated end of trip facilities;
- iii. encouraging the use of travel demand management options that help facilitate the use of public transport, cycling, walking and options to minimise the need to travel; and
- iv. requiring new District Centres to provide opportunities for a public transport interchange.
- v. encouraging the formation of new Central City lanes and upgrading of existing lanes in the Central City, where appropriate, to provide for walking and cycling linkages and public spaces.
- vi. developing a core pedestrian area within the Central City which is compact, convenient and safe, with a wider comprehensive network of pedestrians and cycle linkages that are appropriately sized, direct, legible, prioritized, safe, have high amenity, ensure access for the mobility impaired and are free from encroachment.

Chapter 8 Subdivision, Development and Earthworks

8.2.4 Objective - Earthworks

a. Earthworks facilitate subdivision, use and development, the provision of utilities, hazard mitigation and the recovery of the district.

a. 8.2.4.1 Policy - Water quality

Ensure earthworks do not result in erosion, inundation or siltation, and do not have an adverse effect on surface water or groundwater quality.

8.2.4.3 Policy - Benefits of earthworks

The proposed earthworks are for the purpose of facilitating the development of the site, including establishing the roof top gardens. The earthworks will be suitably managed during the construction period to ensure that adverse effects with regard to sediment runoff, dust, noise and traffic are suitably avoided or mitigated. The nature of the subject site and shallow scale of the proposed earthworks means that any land stability effects are unlikely, noting that the site is generally flat.

Resource consent will be sought separately for disturbance of contaminated soils, should contamination levels within the site prove to be



a. Recognise that earthworks are necessary for subdivision, use and development, the provision of utilities, hazard mitigation and the recovery of the district.

sufficiently high as to trigger the need for resource consent under the NES for contaminated soils.

8.2.4.4 Policy - Amenity

Overall, the proposal is considered to be consistent with this objective and policies.

a. Ensure, once completed, earthworks do not result in any significant shading, visual impact, loss of privacy or other significant detractor from the amenity values enjoyed by those living or working in the locality.

Chapter 14 - Residential

14.2.1 Objective - Housing supply

a. An increased supply of housing that will:

The proposal will provide new accommodation for at least 41 persons on site, within sheltered accommodation and 6 residential units, to meet the needs of displaced youth for safe and affordable, supported accommodation within the city. Rents will be subsidised, supportive of improving the supply of affordable housing for young people.

i. enable a wide range of housing types, sizes, and densities, in a manner consistent with Objectives 3.3.4(a) and 3.3.7;

ii. meet the diverse needs of the community in the immediate recovery period and longer term, including social housing options; and

iii. assist in improving housing affordability.

The proposal is strongly supportive of this objective.

14.2.1.1 Policy - Housing distribution and density

a. Provide for the following distribution of different areas for residential development, in accordance with the residential zones identified and characterised in Table 14.2.1.1a, in a manner that ensures:

As discussed in the assessment above, the proposal will provide a high quality development, with a housing type that is not otherwise readily available in the city (sheltered and supported housing specifically for youth). The proposal will not achieve an average net density of 50 households per hectare as the sheltered housing component does not meet the definition of a residential unit, however were the equivalent number of bedrooms provided in a mix of one and two bedroom units across the site, the required density would be achieved. Although the scale of the community facility exceeds that anticipated for this location, potential adverse effects on the character and amenity of the surrounding area are able to be mitigated through site and building design and on-site management.

...

ii. high density residential development in the Central City, that achieves an average net density of at least 50 households per hectare for intensification development;

...

Table 14.2.1.1a

Residential Central City Zone:

Located within the Central City, the Residential Central City Zone has been developed to contribute to Christchurch's liveable city values. Providing for a range of housing types, including attractive, high density living opportunities, the zone utilises the potential for living, working and playing in close proximity to the commercial centre of the city. The character, scale and intensity of non-residential activities is controlled in order to mitigate effects on the character and amenity of the inner city residential areas.

The proposal is considered to be not contrary to this policy.

Policy 14.2.1.3 Residential development in the Central City

a. To restore and enhance residential activity in the Central City by:

The proposal will introduce residential activity onto a former non-residential site. The housing type proposed will meet an identified need within the community and will result in an increase in the resident population in the central city. Potential adverse effects on the amenity of the existing neighbourhood are able to be managed. The site is to be comprehensively redeveloped, as discussed in the Design Statement in Appendix 9.

i. providing flexibility for a variety of housing types which are suitable for a range of individual housing needs;

ii. providing for a progressive increase in the residential population of the Central City in support of Policy 14.2.1.1.a.ii.;

iii. assisting in the creation of new inner city residential neighbourhoods and the protection of amenity of inner city residential neighbourhoods; and

iv. encourage the comprehensive redevelopment of sites that are no longer required for non-residential purposes.

The proposal is consistent with this policy.



14.2.1.7 Policy - Non-household residential accommodation

a. Enable sheltered housing, refuges, and student hostels to locate throughout residential areas, provided that the building scale, massing, and layout is compatible with the anticipated character of any surrounding residential environment.

An important component of the proposal is the sheltered housing that will be located within the western wing of the site. The scale of the building in the western wing is consistent with the scale of other medium density development in the area, being predominantly two storey and with generous amounts of landscaping and both outdoor and indoor living space available. The sheltered housing proposal is considered to be consistent with this policy.

14.2.4 Objective - High quality residential environments

a. High quality, sustainable, residential neighbourhoods which are well designed, have a high level of amenity, enhance local character and reflect the Ngāi Tahu heritage of Ōtautahi.

Note: Policies 14.2.6.1, 14.2.6.2, 14.2.6.3, and 14.2.6.6 also implement Objective 14.2.4.

As discussed in the assessment above, the proposal is considered to provide a high quality built form that will maintain and enhance the local character and amenity. Extensive courtyards and landscaping will provide a high quality living environment. In addition to the community facility, café and market garden activity on site, the proposal will introduce new residential activity on a formerly non-residential site. Engagement with iwi to ensure that the design engages with Whakapapa and Whenua has been fundamental to the development process. The design proposal has a strong sense of entry to the site, with reference to Māori and Pacific architecture in the entrances, along with a sense of procession and readiness for pōwhiri. The proposal is consistent with this objective.

14.2.4.1 Policy - Neighbourhood character, amenity and safety

a. Facilitate the contribution of individual developments to high quality residential environments in all residential areas (as characterised in Table 14.2.1.1a), through design:

- i. reflecting the context, character, and scale of building anticipated in the neighbourhood;*
- ii. contributing to a high quality street scene;*
- iii. providing a high level of on-site amenity;*
- iv. minimising noise effects from traffic, railway activity, and other sources where necessary to protect residential amenity;*
- v. providing safe, efficient, and easily accessible movement for pedestrians, cyclists, and vehicles; and*
- vi. incorporating principles of crime prevention through environmental design.*

The proposal is not inconsistent with Policy 14.2.4.1 on the basis that the subject site:

- i. will provide residential accommodation that is most appropriately located within residential zones*
- ii. is visually contained, screened and softened to ensure that the non-residential activities do not dominate the residential zone, including through the location of the larger scaled buildings centrally within the site and smaller scale residential activity and entrance features on the periphery of the site;*
- iii. will provide a high quality of built form and landscaping, ensuring a high quality street scene is provided, and will be a considerable enhancement of the street scene compared to the current state of the site;*
- iv. will provide a high level of on-site amenity, with generous communal living and recreation space and landscaping;*
- v. all residential areas well separated from potential traffic noise from Salisbury Street;*
- vi. an emphasis on safe access by pedestrian, cyclists and other forms of micro transport;*
- vii. careful consideration of CPTED principles in the design process; and*



viii. does not involve an activity or buildings that will dominate or detract from the predominant residential character sought by the Plan, noting that the Salisbury Street frontage has a character that is already mixed commercial and residential and the current and historic use of the site is wholly non-residential.

14.2.4.2 Policy - High quality, medium density residential development

a. Encourage innovative approaches to comprehensively designed, high quality, medium density residential development, which is attractive to residents, responsive to housing demands, and provides a positive contribution to its environment (while acknowledging the need for increased densities and changes in residential character), through:

- i. consultative planning approaches to identifying particular areas for residential intensification and to defining high quality, built and urban design outcomes for those areas;*
- ii. encouraging and incentivising amalgamation and redevelopment across large-scale residential intensification areas;*
- iii. providing design guidelines to assist developers to achieve high quality, medium density development;*
- iv. considering input from urban design experts into resource consent applications;*
- v. promoting incorporation of low impact urban design elements, energy and water efficiency, and life-stage inclusive and adaptive design; and*
- vi. recognising that built form standards may not always support the best design and efficient use of a site for medium density development, particularly for larger sites.*

The proposal will provide residential accommodation for at least 41 persons on a site that has historically accommodated no residential activity. The site has been comprehensively designed and includes innovative features such as the roof top greenhouse that will be utilised as a market garden and training opportunity for youth. The proposal will respond directly to an unmet housing demand for supported youth living. The proposal has been designed collaboratively with multiple organisations, youth themselves, iwi and architectural, landscape and urban design experts. The Urban Design Panel has confirmed its support for the proposal. Low impact urban design elements are an important aspect of the project and will continue to be developed as detailed design work progresses.

The proposal is consistent with this policy.

14.2.4.4 Policy - Character of low and medium density areas

a. Ensure, consistent with the zone descriptions in Table 14.2.1.1a, that:

...

ii. medium density areas are characterised by medium scale and density of buildings with predominantly two or three storeys, including semi-detached and terraced housing and low rise apartments, and landscaping in publicly visible areas, while accepting that access to sunlight and privacy may be limited by the anticipated density of development and that innovative approaches to comprehensively designed, high quality, medium density residential development are also encouraged in accordance with Policy 14.1.4.2.

The proposal is consistent with Policy 14.2.4.4 as it has been demonstrated in previous assessments as having no significant adverse effect on the character or amenity of the surrounding Central City residential zone. The building complies with the height, setback and recession plane standards for the site, with taller buildings located centrally within the site and lower scale activity on the periphery to the extent possible.

14.2.6 Objective - Non-residential activities

a. Residential activities remain the dominant activity in residential zones, whilst also recognising the need to:

- i. provide for community facilities and home occupations which by their nature and character typically need to be located in residential zones; and*
- ii. restrict other non-residential activities, unless the activity has a strategic or operational need to locate within a residential zone or is existing guest accommodation on defined sites.*

The proposal will result in no loss of residential activity as the site has to date been utilised for non-residential activity only. Rather, the site will introduce new residential activity for at least 41 persons.

The development incorporates a community facility that requires a central residential zone location. Centrality is important so that it is accessible to youth from across the city, and a residential zone location is important to allow resident youth to feel part of a community whilst they learn to live respectfully and responsibly within a community, with wrap around services available on site to support that learning. The Youth Hub Trust advise that the desired learning to



be a good citizen and resident cannot occur in a non-residential zone.

The Environment Court⁴ has confirmed that the intent of Objective 14.2.6 (a)(ii) is to restrict non-residential activities unless otherwise provided for in the associated policies. As a central city specific policy 14.2.6.8 addresses non residential activity, sub-clause (a)(ii) of this objective would not apply. Subsequent City Council resource consent decision RMA/2018/1336 has confirmed the applicability of that Environment Court decision. In any event, it is considered that there is an operational and strategic need to locate the proposed facility within a residential zone, as discussed in the assessment of Positive Effects above.

The proposal is therefore considered to be consistent with this objective.

14.2.6.1 Policy - Residential coherence character and amenity

a. Ensure that non-residential activities do not have significant adverse effects on residential coherence, character, and amenity.

Residential coherence on the site will be improved by the introduction of residential activity on a previously entirely non-residential site. Non-residential activity within the site will be managed to ensure no significant adverse effects arise.

The proposal is consistent with this policy.

14.2.6.2 Policy - Community activities and community facilities

a. Enable community activities and community facilities within residential areas to meet community needs and encourage co-location and shared use of community facilities where practicable.

b. Enable larger scale community activities and community facilities within defined arterial locations that:

i. are within walking distance of the Central City and suburban commercial centres;

ii. front onto core public transport routes; and

iii. are not dominated by residential development.

The proposal is supportive of clause (a) in that it will be a community facility to meet the needs of the youth in the community and will involve co-location and shared use of community facilities by various organisations involved in supporting youth.

The site is not located within a defined arterial location, though the site is chosen for its central location, good public transport access, walkable distance to city centre and absence of residential activity on site currently. Clause (b) does not discourage larger scale community facilities in other locations and thus the proposal is consistent with this policy.

14.2.6.3 Policy - Existing non-residential activities

a. Enable existing non-residential activities to continue and support their redevelopment and expansion provided they do not:

i. have a significant adverse effect on the character and amenity of residential zones; or

ii. undermine the potential for residential development consistent with the zone descriptions in Table 14.2.1.1a.

Resource consent has recently been obtained to relocate the Christchurch Bowls Club to Christchurch Park, and the site is therefore superfluous to their needs (and has been sold to Anglican Care). Nevertheless, the policy indicates that non-residential activity (the Bowls Club) is enabled to continue and expand on this site. The proposal is neutral in regard to this policy.

14.2.6.4 Policy - Other non-residential activities

a. Restrict the establishment of other non-residential activities, especially those of a commercial or industrial nature, unless the activity has a strategic or operational need to locate within a residential zone, and the effects of such activities on the character and amenity of residential zones are insignificant.

As per the reference to the Environment Court case above and confirmed in RMA/2018/1336 (paragraph 2.18), this policy is not relevant to the proposed development as non-residential activity of this nature is otherwise provided for in the policies.

⁴ *Fright vs Christchurch City Council* [2018] NZEnvC 111.



14.2.6.5 Policy - Retailing in residential zones

a. Ensure that small scale retailing, except for retailing permitted as part of a home occupation, is limited in type and location to appropriate corner sites on higher order streets in the road hierarchy.

Retailing is proposed in the development by way of the proposed café, inclusive of the sale of produce from the roof top market garden. The cafe is very much ancillary to the wider facility and not of a large scale, and is intended first and foremost as a training facility for youth. The proposal is not consistent with this policy in so far as the site is not a corner site, but is located on a higher order street in the road hierarchy. Overall, the proposed cafe is considered to be inconsistent with but not contrary to this policy.

14.2.6.8 Policy – Non-residential activities in Central City residential areas

a. Within Central City residential areas:

i. ensure non-residential activities are of a small scale and compatible with residential activities;

ii. ensure non-residential activities are focussed on meeting the needs of the local residential community or depend on the high level of amenity inherent in the Residential Central City Zone;

iii. ensure new non-residential activities do not compromise the role of the Residential Central City Zone, the Central City Business Zone, or the aim of consolidating that area of the Central City or the Central City Mixed Use Zones;

iv. enable the ongoing operation, use and redevelopment of existing fire service facilities; and

v. protect residential amenity by controlling the character, scale and intensity of non-residential activities.

The proposed community facility (inclusive of café) cannot be said to be of a small scale, but inclusive of the residential activities proposed in the west and east wings, it is considered to be compatible with residential activities.

In regard to clause a(ii), the proposal cannot be said to focus on meeting the needs of the local residential community alone, as the Youth Hub will cater for youth in the wider city. The proposal does however depend on the high level of amenity inherent in the Residential Central City Zone, for the reasons set out above relating to the need for a central residential location for the development. It is noted the proposal need only achieve one of the two parts of clause a(ii), as the clause requires a local residential need or dependence on the high level of amenity inherent in the zone.

Regarding clause a(iii), the community facility will not compromise the role of the Residential Central City Zone, noting the substantial residential activity that will be introduced to a site that currently has no residential activity. The role of the Central City Business Zone will not be compromised as the activity, whilst inclusive of non-residential activity, is not commercial in nature (Objective 15.2.6 states the role of the Commercial Central City Business Zone is to be the principal commercial centre for Christchurch District). Potential adverse effects relating to the community facility and ancillary café are able to be avoided or mitigated through location centrally within the site and towards the Salisbury Street frontage, design and management practices, further ensuring that the role of the zone as a primarily residential area is not compromised. For the same reasons, the proposal is able to protect residential amenity, as sought in clause a(v).

Clause a(iv) is not relevant to this application.

Overall, given the large scale of the community facility, the proposal is partially inconsistent with this policy. The proposal is consistent with the policy where it seeks to protect residential amenity and not compromise the role of



the zone. In summary, and noting the Commissioner's conclusions set out in RMA/2018/1336 that:

- the policy seeks multiple outcomes, not all of which are applicable to all non-residential activities; and

- the policy is not intended to require all sub-clauses to be fully satisfied;

the proposal is considered to be partially inconsistent with but not contrary to this policy.

14.2.8 Objective - Central City residential role, built form and amenity

a. A predominantly residential environment offering a range of residential opportunities, including medium to high density living, within the Central City to support the restoration and enhancement of a vibrant city centre;

b. A form of built development in the Residential Central City Zone that enables change to the existing environment, while contributing positively to the amenity and cultural values of the area, and to the health and safety, and quality and enjoyment, for those living within the area.

The proposal provides for residential activity where currently there is none on site, ensuring that the site will support a predominantly residential environment to a greater extent than currently exists. The site will offer medium density sheltered and independent living, in conjunction with the wrap around support services available on site. The high quality of the proposed development will ensure that the site supports the restoration and enhancement of a vibrant city centre.

The built development proposed will be a change to the existing environment when compared to the former bowling club use, that will contribute positively to the amenity and cultural values of the area and health and safety, whilst ensuring that the quality and enjoyment of residents living in the area will be maintained or enhanced.

The proposal is consistent with this objective.

14.2.8.1 Policy - Building heights

a. Provide for different maximum building heights in areas of the Residential Central City Zone with some areas requiring a reduced height compatible with the existing predominant character.

The proposal is lower than the maximum building height standard applicable to the site.

14.2.8.2 Policy - Amenity standards

a. Prescribing minimum standards for residential development which:

- i. are consistent with higher density living;*
- ii. protect amenity values for residents;*
- iii. integrate development with the adjacent and wider neighbourhood;*
- iv. provide for a range of current and future residential needs; and*
- v. recognise cultural values.*

For the reasons set out in the assessment above, the proposal is considered to maintain or enhance the amenity values of the residential environment generally (albeit with potentially minor adverse effects on some neighbouring properties), will provide for a particular residential need, will be integrated into the adjacent and wider neighbourhood, will introduce new residential activity where none exists currently and recognises cultural values. The proposal is considered to be generally consistent with this policy.

-
105. In summary, though the community facility falls to be considered as a non-residential activity in the Central City under Policy 14.2.6.8, it is also subject to Objective 14.2.6 and Policy 14.2.6.2 which are specifically enabling of community facilities and with which the proposal is strongly consistent. Other specific objectives and policies of the Plan are also strongly supportive of the activity proposed, including 14.2.1, 14.2.1.7, 3.3.1 and 3.3.11.



106. The intention of Objective 14.2.6 to maintain the dominance of residential activity will not be compromised by the proposal, residential coherence will be improved, and residential character and amenity will not be significantly adversely affected. Although the proposed facility is not small in scale, it is considered to be appropriate, and compatible with the surrounding residential activity, given: the scale of the site; the large amounts of landscaping and outdoor living proposed; and the sleeving residential activity proposed on the north east and north west boundaries, with most non-residential activity concentrated centrally or towards the Salisbury Street boundary (where amenity is less dominantly residential and is influenced by nearby commercial activities).
107. Therefore, for the reasons set out in **Table 3** and otherwise having regard to the assessment of effects and the existing/consented environment, the proposal is concluded to be not inconsistent with the relevant provisions of the Plan.

Canterbury Regional Policy Statement

108. The Christchurch District Plan gives effect to the Canterbury Regional Policy Statement ('CRPS') and based on the assessment provided above, it is concluded that the proposal is consistent with the CRPS to the limited extent that it is relevant. For completeness, it is noted that the proposal does not entail any matters of regional significance or issues that are specifically addressed in the CRPS.

Relevant Other Matters

Precedent and Plan Integrity

109. As set out by the High Court in *Rodney District Council v Gould*⁵, issues of plan integrity and precedent effect are not mandatory considerations but are matters to which a decision maker may have regard. This position was reiterated by the High Court in *McKenna v Hastings District Council*⁶.
110. The Environment Court has been careful to stress that both precedent effect and plan integrity remain tied to, rather than separate from, the effects of a proposal. As set out by the Environment Court in *Harris v Central Otago District Council*⁷,

'...questions of whether the integrity of the plan will be undermined if a consent is granted are still questions about the accumulative effects of the activity on aspects of the environment as particularised (usually) in the objectives and policies of the district plan.'

111. The Environment Court in this case also emphasises that when considering possible planning precedent effects, each application must be considered with appropriate regard to the individual merits of that application.

⁵ *Rodney District Council v Gould* [2006] NZRMA 217.

⁶ *McKenna v Hastings District Council* (2009) 15 ELRNZ 41.

⁷ *Harris v Central Otago District Council* [2016] NZEnvC 52 at [57]



112. It is noted that mere non-compliance with a Plan does not itself create a precedent effect.⁸ It should also be noted that in determining whether the integrity of the Plan is threatened, the issue of potential precedent is only one factor to be weighed in each case. Therefore, the integrity of a Plan may not be hindered despite the fact there might be an element of precedent from granting an application.⁹
113. In this instance the proposal is considered not contrary to the policies identifying development within the Residential Central City Zone. The effects of the infringements are not found to lead to any significant adverse effect on the matters which the policy seeks to protect (residential character and amenity). The proposed development has been architecturally designed to be coherent with existing development within the area. It is not considered that there will be any adverse cumulative, precedent or plan integrity adverse effects arising from the proposed development.
114. On balance, it is therefore considered that the effectiveness and integrity of the District Plan would not be harmed by the granting of consent.

Consultation

115. As indicated above, significant and on-going consultation has been held with interested parties through the design phase of the project, including all the organisations who seek to locate there, Christchurch youth and Tangata Whenua. All are supportive of the proposal.
116. A pre-application meeting was held on 1 August 2019 with the City Council. The feedback from that meeting has informed the development of the proposal.
117. An Urban Design Panel meeting was held on 3 September 2019. The feedback from that meeting has been incorporated into the proposal, as set out above.
118. An initial meeting was held with immediate neighbours in February 2019 to outline the proposal in broad form. A further meeting was held between the Youth Hub and its advisors, and immediate neighbours and neighbourhood associations, on 12 October 2019. Some residents expressed concern about anticipated noisy activities, future expansion plans and potential loss of residential activity in the wider neighbourhood. Some residents also considered that while it was a worthwhile project, it should be located in a commercial zone. Residents were assured that there were no plans for future expansion (the site, as now proposed, is fully developed). Many steps have since been taken to mitigate noise, notably including the removal of the basketball court that was formerly proposed at roof level, restrictions on hours of operation and visitor numbers for the events centre, and relocation of the events centre more centrally within the site. Further, five new residential units were added to the proposal on the Gracefield Avenue frontage, significantly increasing the residential component of the proposal.

⁸ *Rosscroft Orchards Limited v Waimakariri District Council* EC Christchurch C160/2001, 14 September 2001.

⁹ *Olive Branch Investments Limited v Central Otago District Council* EC Christchurch C45/2005, 7 April 2005.



Consideration of Alternatives

119. The preceding assessment of effects shows that the proposal will not have any significant adverse effects on the environment. Therefore, an assessment of alternatives is not required.
120. As discussed in the assessment of positive effects above, it is reiterated that the applicant is strongly of the opinion that a central residential site is crucial if the Youth Hub is to succeed in achieving all its objectives. There are likely to be few central residential sites of a size required, available for development by the Youth Hub. For the reasons set out above, commercial or industrial zone locations would not be appropriate.

Mitigation Measures

121. Specific mitigation measures are set out in the assessment above, and include:
- large areas of landscaping, in excess of the 20% required by the District Plan;
 - provision for resident supervisors and an on-site manager;
 - requirement for Events and Traffic Management Plans to be prepared, as a condition of consent; and
 - acoustic mitigation measures.

Monitoring

122. The scale and significance of the activity's effects are such that specific monitoring (beyond Council's standard consent monitoring programme) is not considered to be necessary.

Hazardous substances & installations

123. This application does not propose the use of hazardous substances and installations.

Resource Management Act 1991

RMA Section 104D Assessment

124. As a Non-Complying Activity the application has been assessed against Section 104D of the Resource Management Act 1991. In order for consent to be granted, Council must be satisfied that either the adverse effects of the activity on the environment will be minor, or that the application is for an activity that will not be contrary to the objectives and policies of the District Plan.
125. This assessment concludes that the environmental effects of the proposal will generally be minor, therefore application satisfies the section 104D(1)(a) test.
126. The application has been assessed against the relevant Objectives and Policies of the District Plan as outlined above. This assessment demonstrates that the proposed



development is not contrary to the relevant objectives and policies of the District Plan. This assessment concludes that the proposal satisfies the test contained in section 104D(1)(b) on the basis that the proposal will not be contrary to the objectives and policies of the District Plan.

127. It is therefore considered that the application satisfies both limbs of the Section 104D tests, and consent may therefore be granted.

Part 2 (sections 5-8)

128. In considering an application for resource consent, pre-eminence must be given to Part II, the purpose and principles of the Resource Management Act. The purpose of the Act is to promote the sustainable management of natural and physical resources. Section 5 imposes a duty on consent authorities to promote sustainable management while avoiding, remedying or mitigating adverse effects of activities on the environment. In this respect, the proposal is consistent with the enabling provision of section 5 in that the proposal will provide for the efficient and sustainable use of the site and for the wellbeing of the existing and future community. Importantly, the activity will not result in any adverse effects that would be in conflict with section 5(2)(a) – (c).
129. There are no section 6 (matters of national importance) or section 8 matters (Treaty of Waitangi) which need to be taken into account.
130. Section 7 lists various matters to which regard shall be had in achieving the purpose of the Act. The matters of particular relevance to this application are:
- (b) *the efficient use and development of natural and physical resources;*
 - (c) *the maintenance and enhancement of amenity values; and*
 - (f) *the maintenance and enhancement of the quality of the environment.*
131. In respect of subsections (c) and (f), based on the assessment of effects provided earlier, the proposal will maintain amenity values and the quality of the environment. In terms of subsection (b), the proposal entails efficient use and development of the physical land resource.
132. In summary, the proposal is in keeping with Part II of the Resource Management Act 1991.

Recovery Strategy & Plans

133. In determining the application, Council must be satisfied that the application is not inconsistent with relevant recovery strategies and plans. The application site is located within an urban area as identified by the Land Use Recovery Plan ('LURP'), and the approval of this application will support the 'Rebuilding of Communities' sought by the LURP. Accordingly, the proposal is consistent with the LURP.
134. Directly relevant is the Christchurch Central Recovery Plan – Te Mahere Maraka Otautahi ('CCRP'). The CCRP seeks certainty for the central city in support of earthquake recovery. The site is not located within one of the precincts or sites specifically identified in the plan and its accompanying Blueprint. The proposal is supportive of the CCRP in that it provides



for the comprehensive redevelopment of an earthquake damaged site, with provision for both residential and community facilities. Access to the site will be encouraged by modes of transport other than cars, with large amounts of cycle parking available on site, and good accessibility to public transport.

135. Overall, the proposal is considered to be consistent with the CCRP.

Conclusion

136. The application is consistent with relevant recovery strategies and plans.
137. The proposal is consistent with the purpose and principles of the Act in that it enables people to provide for their economic and social well-being, while maintaining and enhancing the quality and amenity of the local environment and avoiding significant adverse effects.
138. The proposal is not inconsistent with the relevant objectives and policies of the Christchurch District Plan, and will have actual or potential effects on the environment which are acceptable and no more than minor.
139. Overall, it is considered that consent ought to be granted in accordance with sections 104, 104B and 104D of the Act, subject to appropriate conditions.



Appendix 1

Certificate of Title



**RECORD OF TITLE
UNDER LAND TRANSFER ACT 2017
FREEHOLD
Search Copy**




R. W. Muir
Registrar-General
of Land

Identifier **889770**
Land Registration District **Canterbury**
Date Issued 02 October 2019

Prior References

CB38B/1084

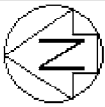
Estate Fee Simple
Area 4250 square metres more or less
Legal Description Lot 7 Deposited Plan 536430

Registered Owners

Social Service Council of the Diocese of Christchurch

Interests

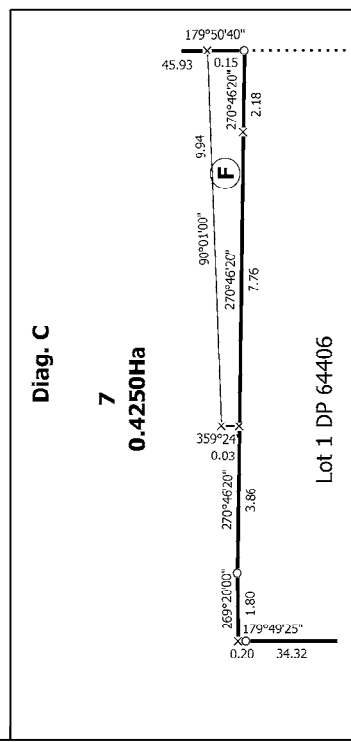
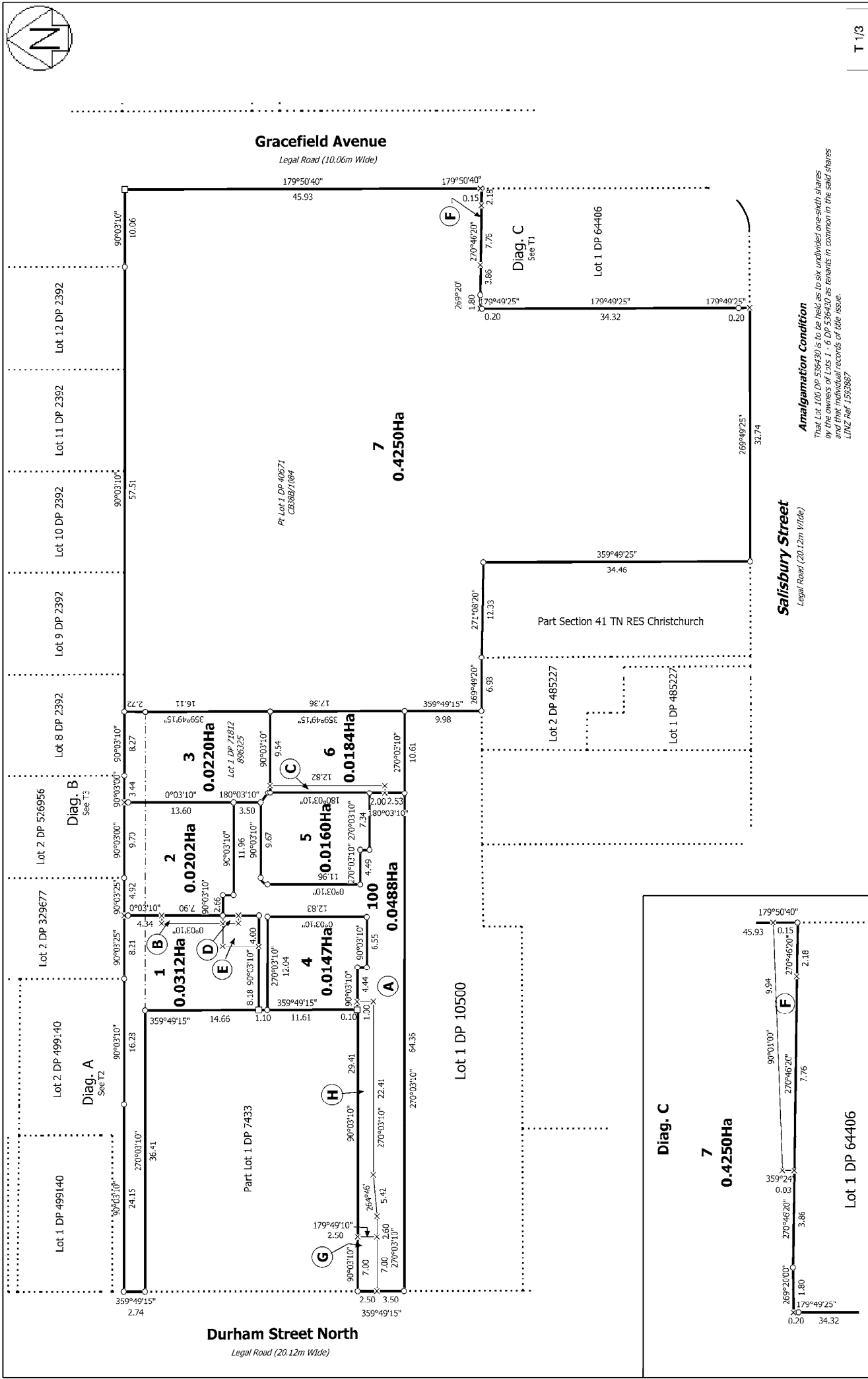
1276 Order in Council imposing Building Line Restriction - 16.10.1933 at 10:00 am (affects Gracefield Avenue)
Appurtenant hereto is a party wall easement created by Transfer A100064.5 - 4.3.1994 at 11:20 am
Subject to a party wall easement over part marked F on DP 536430 created by Transfer A100064.5 - 4.3.1994 at 11:20 am
11457638.8 Consent Notice pursuant to Section 221 Resource Management Act 1991 - 2.10.2019 at 10:51 am
11457638.9 Encumbrance to the Christchurch City Council - 2.10.2019 at 10:51 am



Gracefield Avenue
Legal Road (10.06m Wide)

Durham Street North
Legal Road (20.12m Wide)

Salisbury Street
Legal Road (20.12m Wide)



Amalgamation Condition
That Lot 100 DP 536430 is to be held as to six undivided one-sixth shares by the owners of Lots 1 - 6 DP 536430 as tenants in common in the said shares and that individual records of title issue.
LINZ Ref: 1583887

T 1/3

<p>Land District: Canterbury Digitally Generated Plan Generated on: 11/10/2019 10:37am Page 4 of 6</p>	<p>LOTS 1 - 7 & 100 BEING A SUBDIVISION OF LOT 1 DP 71812 & Pt LOT 1 DP 40671</p>	<p>Surveyor: Christopher Paul Hopper Firm: Davis Ogilvie & Partners Ltd (Christcl)</p>	<p>Title Plan LT 536430 Approved on: 11/10/2019</p>
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View Instrument Details

Instrument No. 11457638.8
Status Registered
Date & Time Lodged 02 Oct 2019 10:51
Lodged By Glausius, Lucy Jane Evelyn
Instrument Type Consent Notice under s221(4)(a) Resource Management Act 1991



Affected Records of Title	Land District
889770	Canterbury

Annexure Schedule Contains 1 Pages.

Signature

Signed by Lucy Jane Evelyn Glausius as Territorial Authority Representative on 25/09/2019 01:36 PM

***** End of Report *****



IN THE MATTER OF the Resource Management Act 1991

AND

IN THE MATTER OF DP 536439 and Subdivision Consent RMA/2019/896

CONSENT NOTICE PURSUANT TO SECTION 221, RESOURCE MANAGEMENT ACT 1991

To: The Registrar-General of Land
Canterbury Land Registration District
LAND INFORMATION NEW ZEALAND

TAKE NOTICE that the land described below is subject to conditions in relation to a subdivision consent as follows:

Specific Foundation Design Lot 7

Any further development of Lot 7 will require a specific foundation design by a chartered engineer or by an appropriately qualified geotechnical engineer

AND THAT you are hereby directed and required to register the same pursuant to Section 221 Resource Management Act 1991.

DESCRIPTION OF LAND AFFECTED

ALL THAT piece of land comprising:

- Lot 7 DP 536430 comprised in Record of Title 889770

DATED this 8th day of July 2019

SIGNED for and on behalf of
CHRISTCHURCH CITY COUNCIL

A handwritten signature in black ink, appearing to read "Mal Nash".

Mal Nash
Team Leader Planning

Authorised Officer (for the purposes of Section 221 Resource Management Act 1991)



Appendix 2

Application Plans



Resource Consent Application RFI 01

Te Hurihanga Ō Rangatahi The Youth Hub

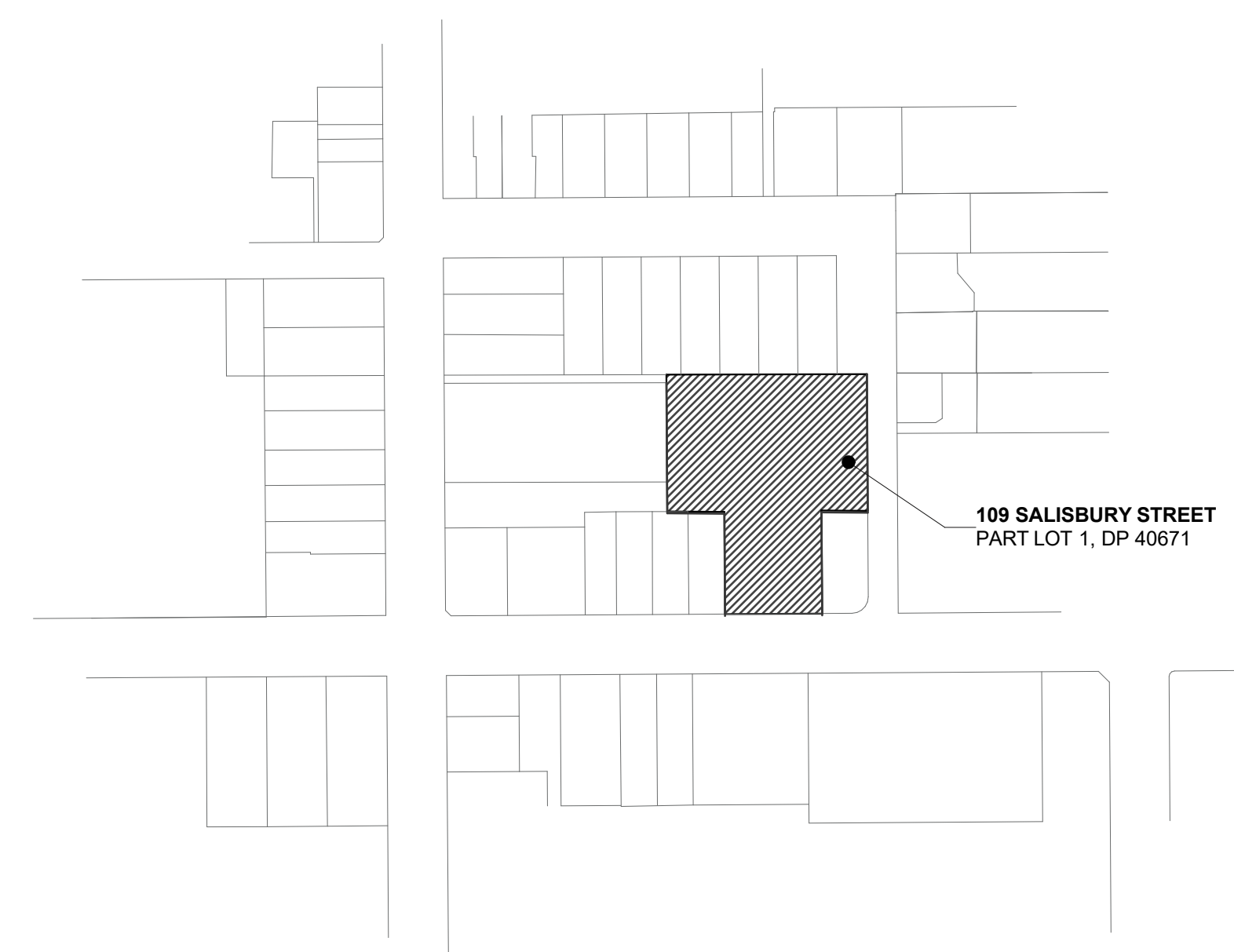
109 Salisbury Street Christchurch New Zealand

Sheet Index					
Layout ID	Layout Name	Published	Issued Date	Current Revision ID	Issue Name
A000	Cover Sheet	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A001	Indicative 3D Images	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A002	Indicative 3D Images	<input checked="" type="checkbox"/>	24/02/2020 9:57 AM	01	Resource Consent
A101	Proposed Site plan	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A200	Ground Floor Plan	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A201	First Floor Plan	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A202	Second Floor Plan	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A203	Roof Plan	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A300	Elevations	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A301	Elevations	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A900	Project Zones	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A901	GFA - Ground Floor	<input checked="" type="checkbox"/>	24/02/2020 9:57 AM	01	Resource Consent
A902	GFA - First Floor	<input checked="" type="checkbox"/>	24/02/2020 9:57 AM	01	Resource Consent
A903	GFA - Second Floor	<input checked="" type="checkbox"/>	22/05/2020 5:21 PM	02	RFI1
A904	Public Area - Ground Floor	<input checked="" type="checkbox"/>	24/02/2020 9:57 AM	01	Resource Consent
A905	Public Area - First Floor	<input checked="" type="checkbox"/>	24/02/2020 9:57 AM	01	Resource Consent
A906	Public Area - Second Floor	<input checked="" type="checkbox"/>	24/02/2020 9:57 AM	01	Resource Consent

CH-23

Distribution	
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Local Authorities	<input checked="" type="checkbox"/>
Structural Engineer	<input type="checkbox"/>
Landscape Architect	<input type="checkbox"/>
Planner	<input checked="" type="checkbox"/>
Urban Designer	<input type="checkbox"/>
Traffic Engineer	<input type="checkbox"/>
Acoustic Engineer	<input type="checkbox"/>

Distribution Format		
Electronic	PDF	<input checked="" type="checkbox"/>
	ArchiCAD	<input type="checkbox"/>
Physical	Paper	<input type="checkbox"/>



1. Site Location Plan 1:2000

RevID	CHD	Change Name	Date
01			24/02/2020
02	CH-23	Cover sheet updated	22/05/2020

NOT FOR CONSTRUCTION

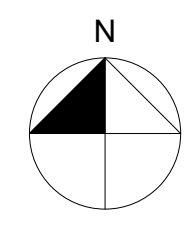
Do not scale.
The contractor shall verify all dimensions before commencing work, and all discrepancies reported to Field Ltd. for clarification.



Client
The Youth Hub Trust

Project
Te Hurihanga Ō Rangatahi - The Youth Hub
109 Salisbury Street Christchurch New Zealand

Title
Cover Sheet



Scale	1:2000 @ A1
Drawn	IN/JK
Checked	AK/AJ
Project no.	18-008
Drawing no.	A000
Rev.	02



View of Salisbury Street entrance



View of Gracefield Ave. entrance

CH-28

RevID	CHID	Change Name	Date
01			24/02/2020
02	CH-28	Image updated	23/05/2020

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Boxed Quarter, 270 St. Asaph Street, Christchurch, New Zealand

Client
The Youth Hub Trust

Project
Te Hurihanga Ō Rangatahi - The Youth Hub
109 Salisbury Street Christchurch New Zealand

Indicative 3D Images

Scale
@ A1

Drawn
IN/JK

Checked
AK/AJ

Project no.	Drawing no.	Rev.
18-008	A001	02



Over view of site from south east



Over view of site from north west

RevID	CHD	Change Name	Date
01			24/02/2020

NOT FOR CONSTRUCTION

Do not scale. The contractor shall verify all dimensions before commencing work, and all discrepancies reported to Field Ltd. for clarification.

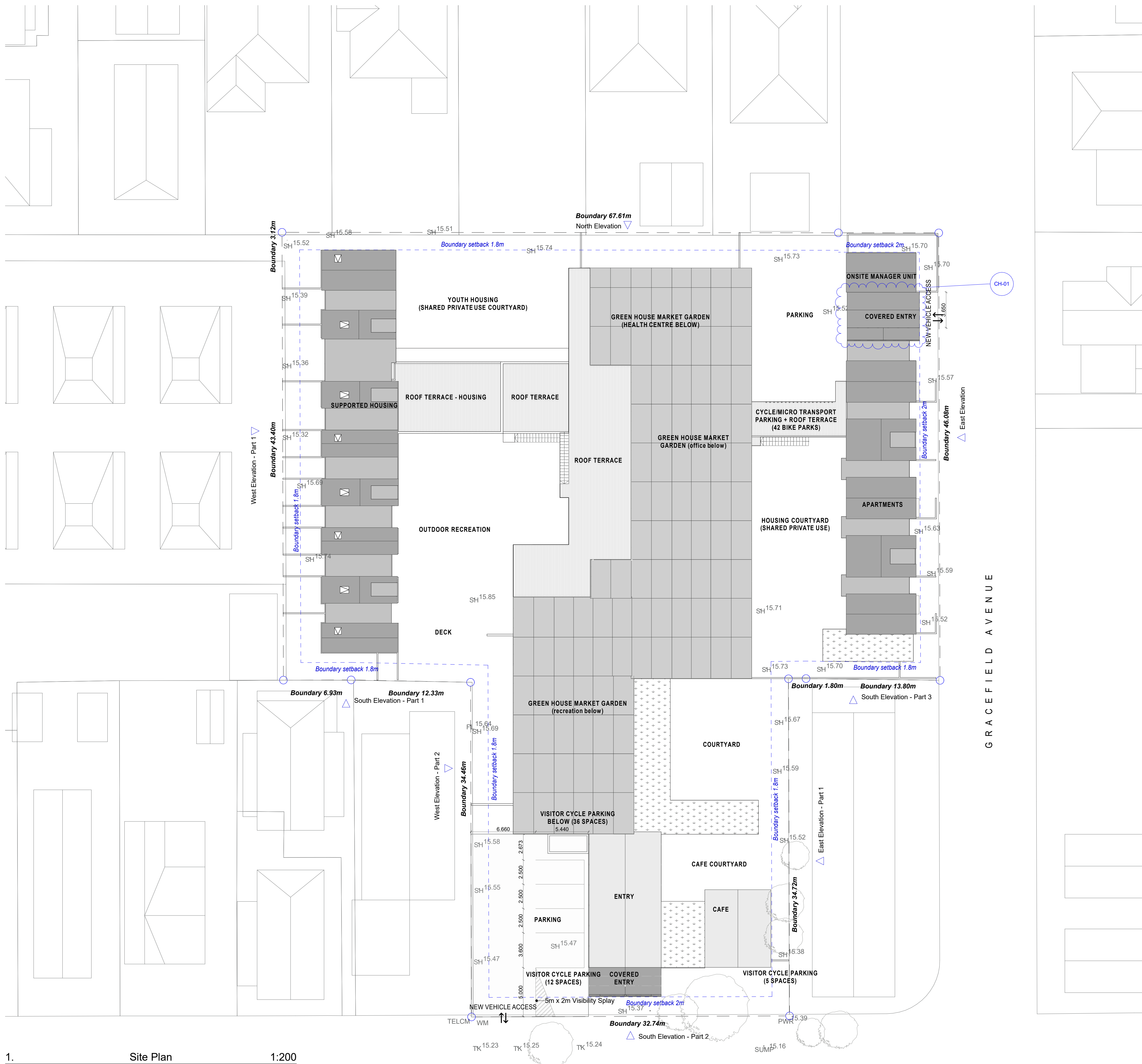

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Client
 The Youth Hub Trust
Project
Te Hurihanga Ō Rangatahi - The Youth Hub
 109 Salisbury Street Christchurch New Zealand

Indicative 3D Images

Scale
 @ A1
Drawn
 IN/JK
Checked
 AK/AJ

Project no.	Drawing no.	Rev.
18-008	A002	01



BUILDING FLOOR AREAS

Residential Units	5 units: 4 x 3 bedroom unit 1 x 4 bedroom unit (includes 1 x accessible bedroom)
Gross floor area	503.55m ²
On site managers unit	42.75m ²
Residential Activity (Youth hub housing):	22 housing units 2 supervisor units
Gross floor area	462.31m ²
Food and Beverage (cafe)	Gross floor area 68.32m ² Public area 48.95m ² Outdoor Courtyard 69.92m ²

SITE PLAN LEGEND

- Boundary
- Proposed Tree (Refer to landscape architects documents to planting details)
- Vehicle Access
- Surveyed levels
- Greenhouse (glass) roofs
- Walk-on terrace with membrane below
- Possible green roof with membrane below
- Long-run steel roofing (Kowhai Glow)
- Membrane roof

SITE INFORMATION

Site Description:	Pt Lot 1 DP 40671
CCC Zone:	Residential Central City
Site Area:	4250m ²
Building Footprint:	2236.5m ²
Site Coverage:	52.62%

BUILDING AREA SITE COVERAGE

Building Footprint:	2236.5m ²	52.62%
Parking:	389.35m ²	9.16%

Refer to Landscape Architects documents to Hardscaping and Landscaping coverage information

CYCLE PARKING

Covered Visitor	36 spaces
Uncovered Visitor	17 spaces
Staff	42 spaces

SURVEY INFORMATION:

Budget Setouts LTD
28-06-19
Heights in terms of Chch Drainage Datum

RevID	CHD	Change Name	Date
01			24/02/2020
02	CH-01	Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted.	22/05/2020

NOT FOR CONSTRUCTION

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Boxed Quarter, 270 St. Asaph Street, Christchurch, New Zealand

Client: The Youth Hub Trust
Project: Te Hurihanga Ō Rangatahi - The Youth Hub
109 Salisbury Street Christchurch New Zealand

Proposed Site plan

Scale:	1:200 @ A1
Drawn:	IN/JK
Checked:	AK/AJ
Project no.:	18-008
Drawing no.:	A101
Rev.:	02



FLOOR PLAN LEGEND

- Boundary
- Setback Line
- Roof Above/Below
- Void

SPACE DESIGNATIONS

- AW** Accessible toilet
- AWS** Accessible toilet with shower
- AU** Accessible single sleeping unit with ensuite
- B** Bed room
- BS** Bin store
- BT** Bathroom
- CT** Consult room
- CU** On-site manager unit
- D** Dining
- K** Kitchen
- KT** Kitchenette
- L** Living
- LA** Laundry facilities
- LT** Lift
- M** Meeting
- O** Office/Breakout
- PC** Private courtyard
- PCU** Residential 3 bedroom unit
- R4U** Residential 4 bedroom unit
- S** Storage
- ST** Study
- SU** Supervisor sleeping unit
- U** Single sleeping unit with ensuite
- W** Unisex toilet

RevID	CHID	Change Name	Date
01	CH-01	Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted.	24/02/2020
	CH-02	Glazing added	
	CH-05	Doors added	
	CH-07	Glazing adjusted	
	CH-12	Glazing moved	
02	CH-17	Wall moved south	22/05/2020
	CH-18	Interior layout adjusted	
	CH-19	Bin store added	
	CH-24	Dimension added	
	CH-25	Shown for clarity	
	CH-26	Door arrangement changed	

NOT FOR CONSTRUCTION

Do not scale. The contractor shall verify all dimensions before commencing work, and all discrepancies reported to Field Ltd. for clarification.



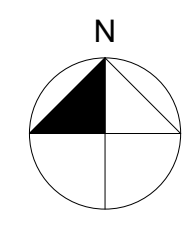
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 | 021 184 5191 | info@fieldstudio.co.nz |
 Boxed Quarter, 270 St. Asaph Street, Christchurch, New Zealand

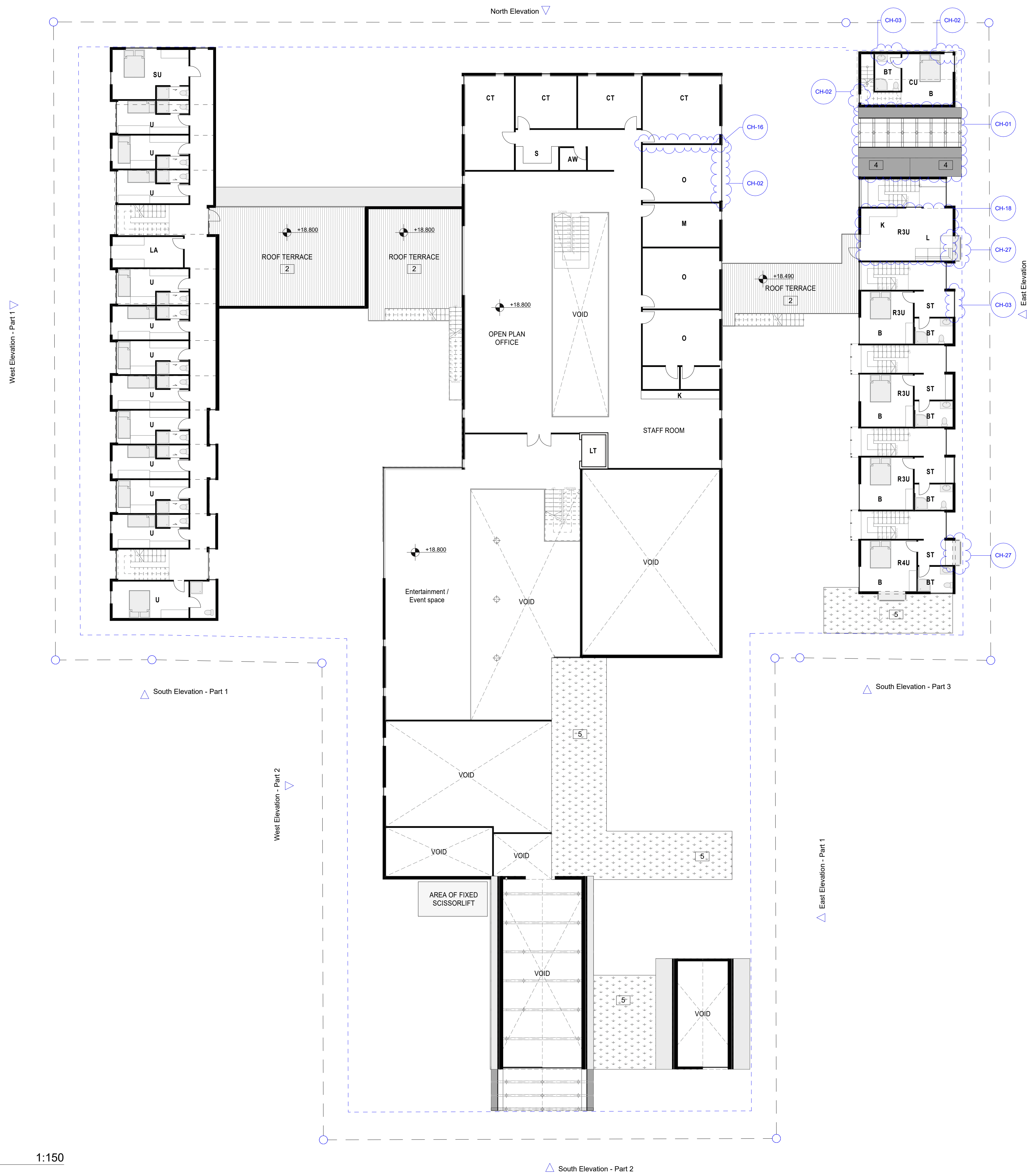
Client
The Youth Hub Trust
 Project
Te Hurihanga Ō Rangatahi - The Youth Hub
 109 Salisbury Street Christchurch New Zealand

Ground Floor Plan

Scale
1:150 @ A1
 Drawn
IN/JK
 Checked
AK/AJ

Project no. 18-008
 Drawing no. **A200**
 Rev. 02





- ROOF PLAN LEGEND**
- Roof Pitch
 - 1 Greenhouse (glass) roofs
 - 2 Walk-on terrace with membrane below
 - 3a Long-run steel roofing (Kowhai Glow)
 - 3b Long-run steel roofing (galvanised)
 - 4 Membrane roof
 - 5 Possible green roof with membrane below
 - 6 Translucent profiled roofing
 - 7 Timber Decking

- FLOOR PLAN LEGEND**
- Boundary
 - - - Setback Line
 - - - Roof Above/Below
 - ⊠ Void
- SPACE DESIGNATIONS**
- AW Accessible toilet
 - AWS Accessible toilet with shower
 - AU Accessible single sleeping unit with ensuite
 - B Bed room
 - BS Bin store
 - BT Bathroom
 - CT Consult room
 - CU On-site manager unit
 - D Dining
 - K Kitchen
 - KT Kitchenette
 - L Living
 - LA Laundry facilities
 - LT Lift
 - M Meeting
 - O Office/Breakout
 - PC Private courtyard
 - R3U Residential 3 bedroom unit
 - R4U Residential 4 bedroom unit
 - S Storage
 - ST Study
 - SU Supervisor sleeping unit
 - U Single sleeping unit with ensuite
 - W Unisex toilet

RevID	CHID	Change Name	Date
01			24/02/2020
	CH-01	Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted.	
	CH-02	Glazing added	
02	CH-03	Window visibility updated for clarity	22/05/2020
	CH-16	Wall moved north	
	CH-18	Interior layout adjusted	
	CH-19	Bin store added	
	CH-27	New bay window	

NOT FOR CONSTRUCTION

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 The Youth Hub Trust

Project
 Te Hurihanga Ō Rangatahi - The Youth Hub
 109 Salisbury Street Christchurch New Zealand

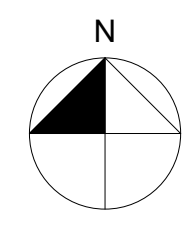
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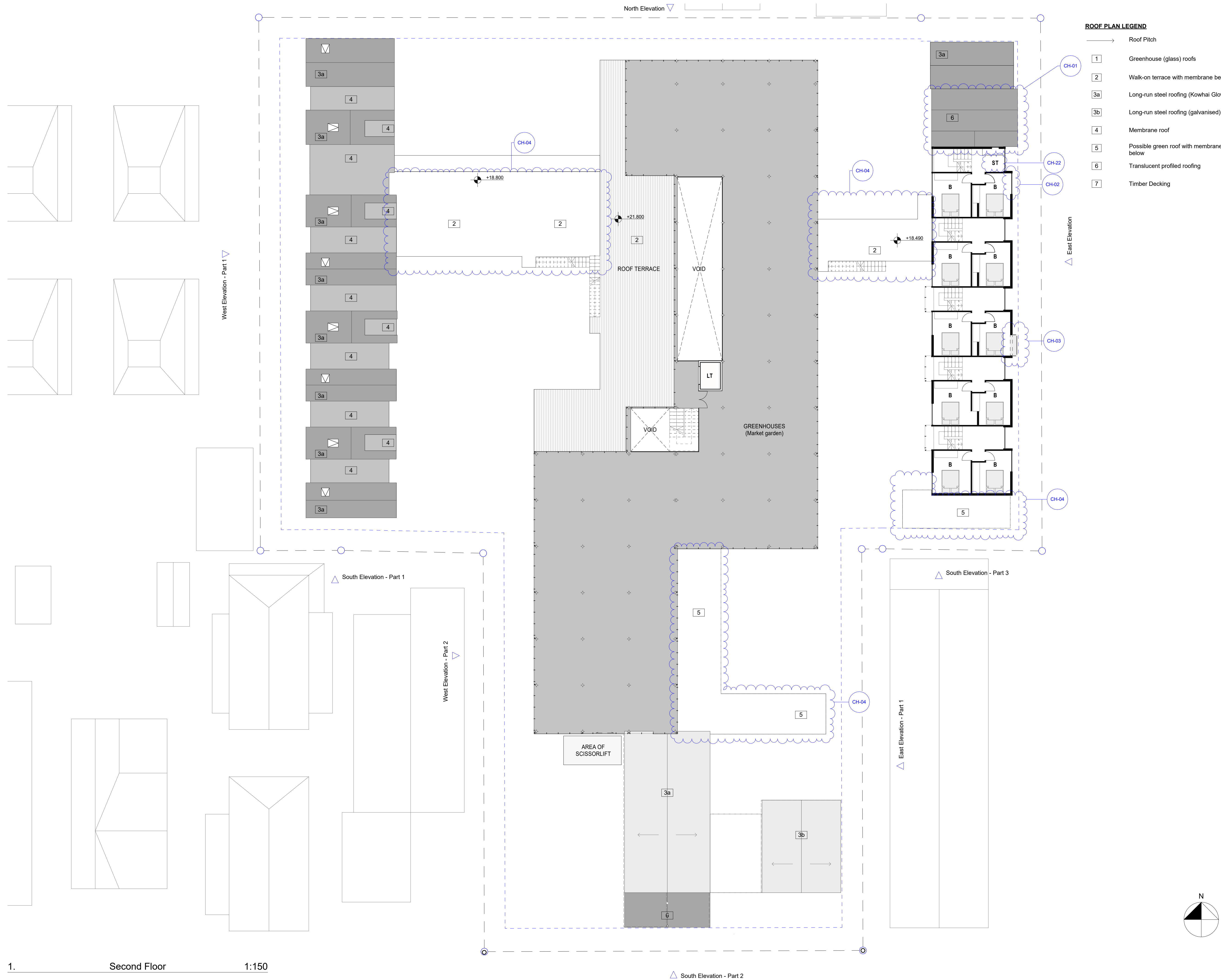
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Drawn
 IN/JK

Checked
 AK/AJ

Project no.	Drawing no.	Rev.
18-008	A201	02





ROOF PLAN LEGEND

- Roof Pitch
- 1 Greenhouse (glass) roofs
- 2 Walk-on terrace with membrane below
- 3a Long-run steel roofing (Kowhai Glow)
- 3b Long-run steel roofing (galvanised)
- 4 Membrane roof
- 5 Possible green roof with membrane below
- 6 Translucent profiled roofing
- 7 Timber Decking

FLOOR PLAN LEGEND

- Boundary
 - - - Setback Line
 - - - Roof Above/Below
 - ⊠ Void
- SPACE DESIGNATIONS**
- AW Accessible toilet
 - AWS Accessible toilet with shower
 - AU Accessible single sleeping unit with ensuite
 - B Bed room
 - BS Bin store
 - BT Bathroom
 - CT Consult room
 - CU On-site manager unit
 - D Dining
 - K Kitchen
 - KT Kitchenette
 - L Living
 - LA Laundry facilities
 - LT Lift
 - M Meeting
 - O Office/Breakout
 - PC Private courtyard
 - R3U Residential 3 bedroom unit
 - R4U Residential 4 bedroom unit
 - S Storage
 - ST Study
 - SU Supervisor sleeping unit
 - U Single sleeping unit with ensuite
 - W Unisex toilet

RevID	CHID	Change Name	Date
01			24/02/2020
	CH-01	Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted.	
02	CH-02	Glazing added	22/05/2020
	CH-03	Window visibility updated for clarity	
	CH-04	Surface texture removed for clarity	
	CH-19	Bin store added	
	CH-22	Study added	

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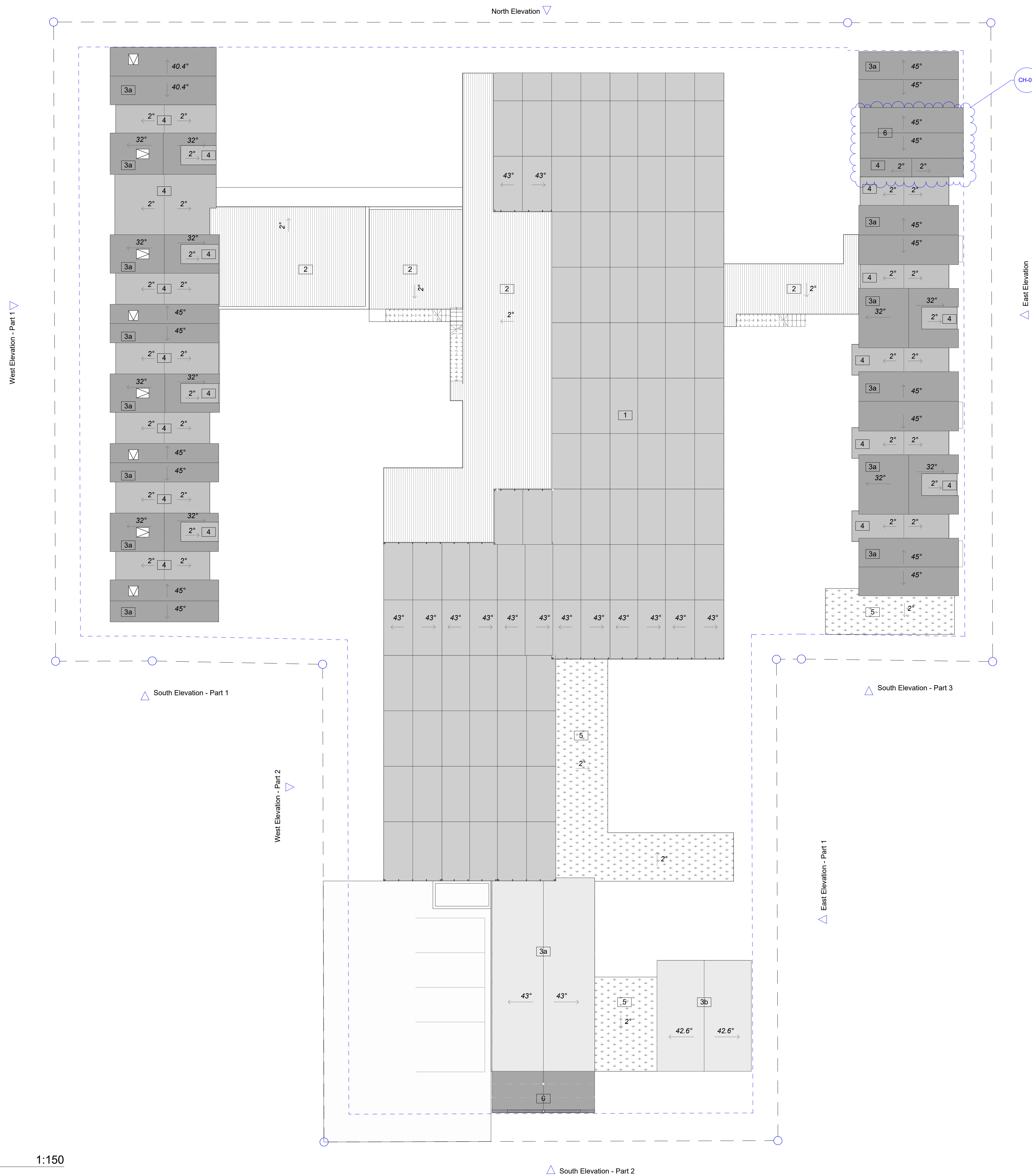
Client
The Youth Hub Trust
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 109 Salisbury Street Christchurch New Zealand

Second Floor Plan

Scale: 1:150 @ A1
 Drawn: IN/JK
 Checked: AK/AJ

Project no.	Drawing no.	Rev.
18-008	A202	02

Project no. 18-008 Drawing no. A202 Rev. 02



ROOF PLAN LEGEND

Code	Description
→	Roof Pitch
1	Greenhouse (glass) roofs
2	Walk-on terrace with membrane below
3a	Long-run steel roofing (Kowhai Glow)
3b	Long-run steel roofing (galvanised)
4	Membrane roof
5	Possible green roof with membrane below
6	Translucent profiled roofing
7	Timber Decking

RevID	CHID	Change Name	Date
01			24/02/2020
02	CH-01	Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted.	22/05/2020

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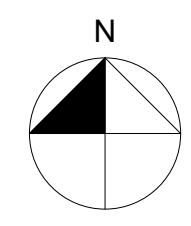
Roof Plan

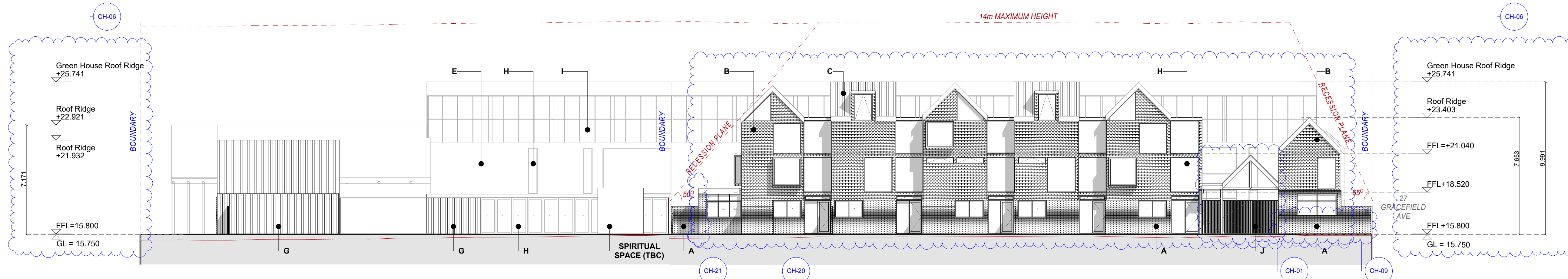
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Drawn
 IN/JK

Checked
 AK/AJ

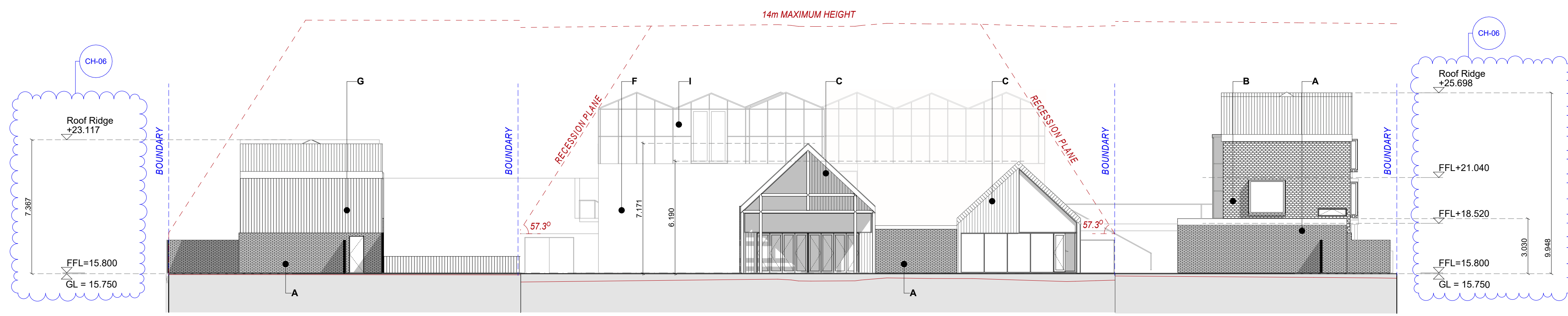
Project no.	Drawing no.	Rev.
18-008	A203	02





1. East Elevation - Part 1 1:150
A101, A200, A201, A202, A203, A907

2. East Elevation - Gracefield Avenue 1:150
A101, A200, A201, A202, A203



3. South Elevation - Part 1 1:150
A101, A200, A201, A202, A203

4. South Elevation - Salisbury Street 1:150
A101, A200, A201, A202, A203

5. South Elevation - Part 3 1:150
A101, A200, A201, A202, A203

- FINISHES LEGEND**
- Refer to 3D images on sheet A001 and A002 for indicative colour and materials
- A Brick (type & colour TBC)
 - B Timber Shingles
 - C Colorsteel Cladding in 'Kowhai Glo'
 - D Galvanised Steel Cladding
 - E Concrete with teal paint finish
 - F Concrete with maroon paint finish
 - G Vertical Timber Cladding
 - H Glazing
 - I Green House (Glazing)
 - J Timber Gate
- Existing ground levels
Proposed ground levels

RevID	CHD	Change Name	Date
01			24/02/2020
CH-01		Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted	
02	CH-06	Building heights added + Building levels corrected	22/05/2020
	CH-09	Fence height reduced to 1200mm	
	CH-20	Facade glazing rearranged	
	CH-21	Portion of building moved south	

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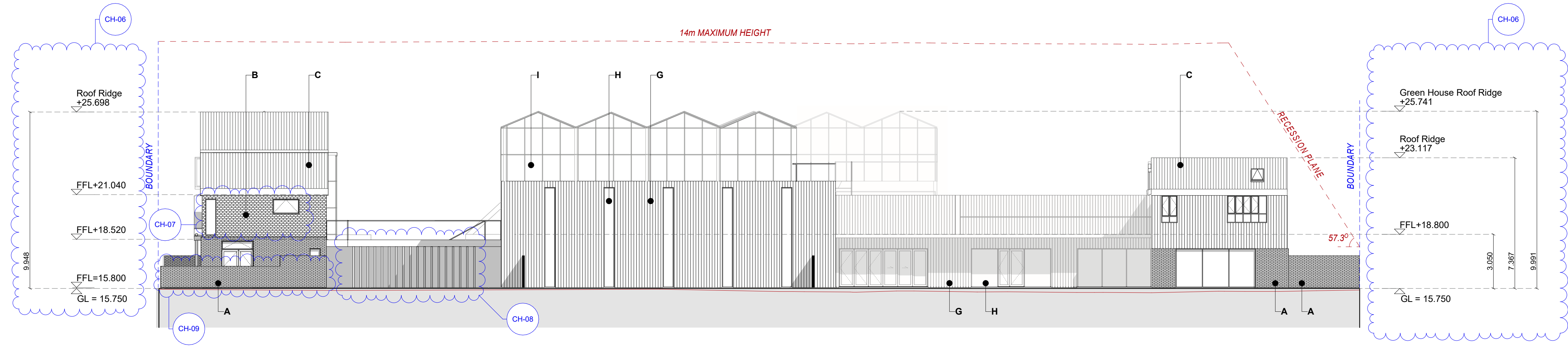
Project
Te Hurihanga Ō Rangatahi - The Youth Hub
109 Salisbury Street Christchurch New Zealand

Title
Elevations

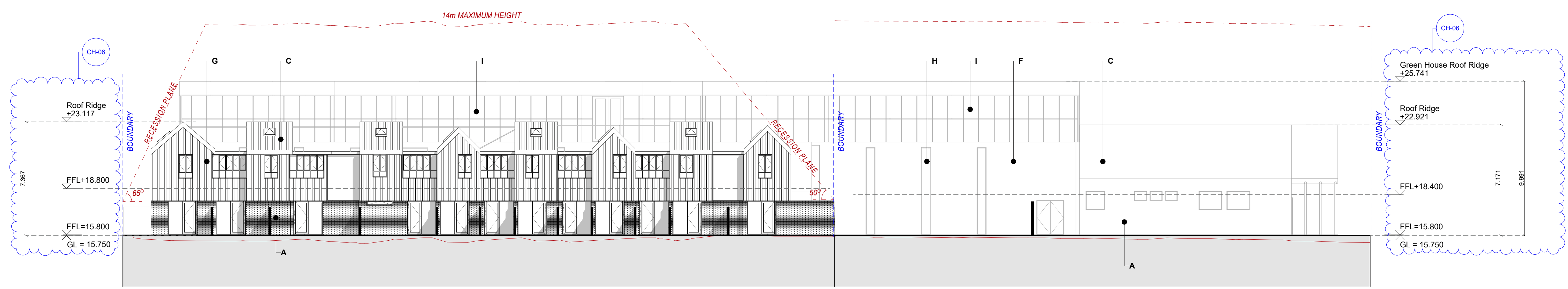
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Drawn
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Checked
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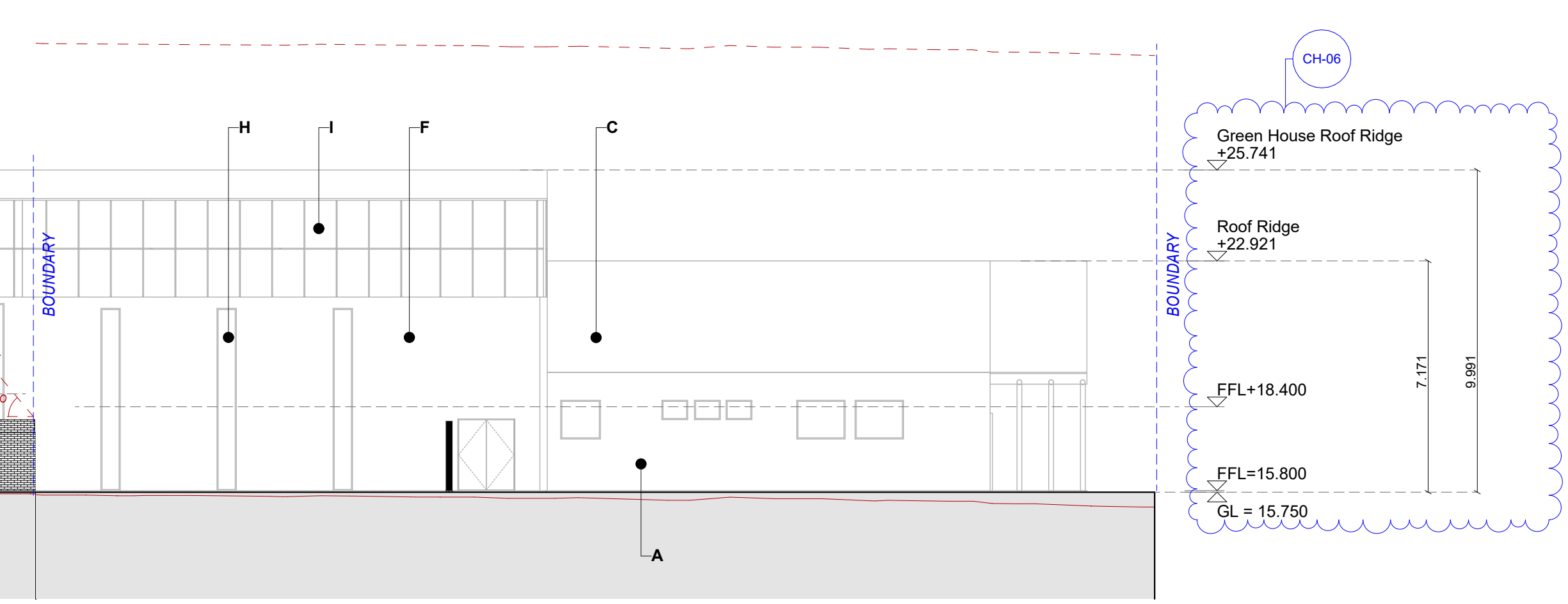
Project no.	Drawing no.	Rev.
18-008	A300	02



1. North Elevation 1:150
A101, A200, A201, A202, A203



2. West Elevation - Part 1 1:150
A101, A200, A201, A202, A203



3. West Elevation - Part 2 1:150
A101, A200, A201, A202, A203

- FINISHES LEGEND**
- Refer to 3D images on sheet A001 and A002 for indicative colour and materials
- A Brick (type & colour TBC)
 - B Timber Shingles
 - C Colorsteel Cladding in 'Kowhai Glo'
 - D Galvanised Steel Cladding
 - E Concrete with teal paint finish
 - F Concrete with maroon paint finish
 - G Vertical Timber Cladding
 - H Glazing
 - I Green House (Glazing)
 - J Timber Gate
- Existing ground levels
— Proposed ground levels

RevID	CHID	Change Name	Date
01			24/02/2020
	CH-06	Building heights added + Building levels corrected	
	CH-07	Glazing adjusted	
02	CH-08	Bike storage separation added	22/05/2020
	CH-09	Fence height reduced to 1200mm	

NOT FOR CONSTRUCTION

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Elevations

Scale
1:150 @ A1

Drawn
IN/JK

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AK/AJ

Publish Date: 22/05/2020
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Project no. 18-008	Drawing no. A301	Rev. 02
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GROSS FLOOR AREA - FIRST FLOOR

- Zone of gross floor area
- Residential Units 170.72m²
- On site managers unit 18.52m²
- Residential Activity 171.59m²
- Food and Beverage (cafe) Nil
- Entertainment / Event space 189.95m²
- Health Care / Counselling 109.87m²
- Support Services 234.17m²
- Basketball Court Nil

NOTE:

This drawing is intended to be read in colour



RevID	CHD	Change Name	Date
01			24/02/2020

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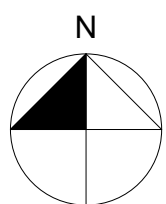
Project
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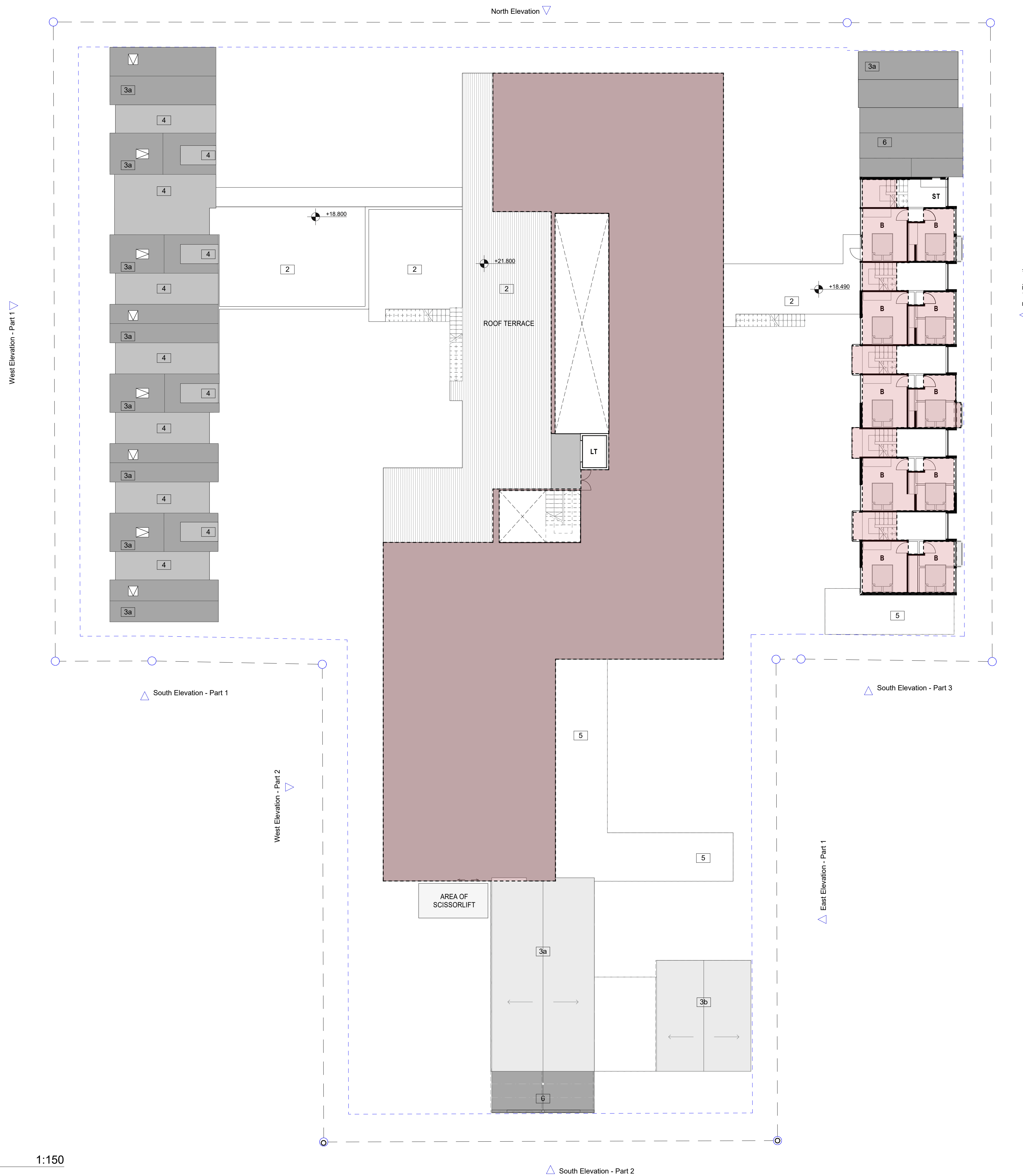
Title
GFA - First Floor

Scale
1:150 @ A1
Drawn
IN/JK
Checked
AK/AJ

Published for
22/05/2020
Issued for
Resource Consent

Project no.	Drawing no.	Rev.
18-008	A902	01





GROSS FLOOR AREA - SECOND FLOOR

	Zone of gross floor area
	Residential Units 152.89m ²
	On site managers unit Nil
	Residential Activity Nil
	Food and Beverage (cafe) Nil
	Entertainment / Event space Nil
	Health Care / Counselling Nil
	Support Services Nil
	Basketball Court Nil

NOTE:

This drawing is intended to be read in colour



RevID	CHID	Change Name	Date
01			24/02/2020
	CH-01	Rearrangement to single 1200mm clear pedestrian/cycle way, vehicle entrance moved north accordingly and covered entrance roof arrangement adjusted.	
02			22/05/2020
	CH-03	Window visibility updated for clarity	
	CH-04	Surface texture removed for clarity	

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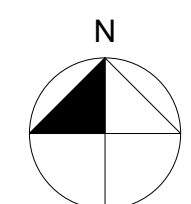
GFA - Second Floor

Scale
1:150 @ A1

Drawn
IN/JK

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Project no. 18-008
Drawing no. **A903**
Rev. 02



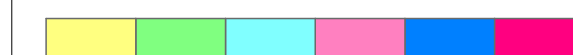


PUBLIC FLOOR AREA - GROUND FLOOR

- Zone of public floor area
- Residential Units
Nil
- On site managers unit
Nil
- Residential Activity
Nil
- Food and Beverage (cafe)
48.95m²
(Outdoor Courtyard = 69.92m²)
Entertainment / Event space
589.09m²
- Health Care / Counselling
103.80m²
- Support Services
91.34m²
- Basketball Court
134.19m²

NOTE:

This drawing is intended to be read in colour



RevID	CHD	Change Name	Date
01			24/02/2020

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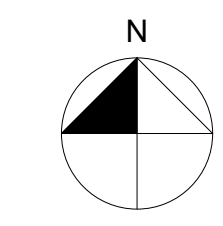
Client
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Project
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Title
Public Area - Ground Floor

Scale: 1:150 @ A1
Drawn: IN/JK
Checked: AK/AJ
Publish Date: 22/05/2020
Project no: 18-008

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Rev: A904
01

1. Ground Floor - Public Area Zones 1:150





PUBLIC FLOOR AREA - FIRST FLOOR

- Zone of public floor area

- Residential Units
Nil

- On site managers unit
Nil

- Residential Activity
Nil

- Food and Beverage (cafe)
Nil

- Entertainment / Event space
138.95m²

- Health Care / Counselling
91.61m²

- Support Services
Nil

- Basketball Court
Nil

NOTE:

This drawing is intended to be read in colour



RevID	CHD	Change Name	Date
01			24/02/2020

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Title
Public Area - First Floor

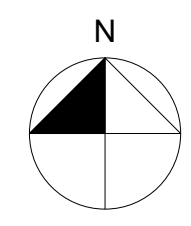
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IN/JK

Checked
AK/AJ

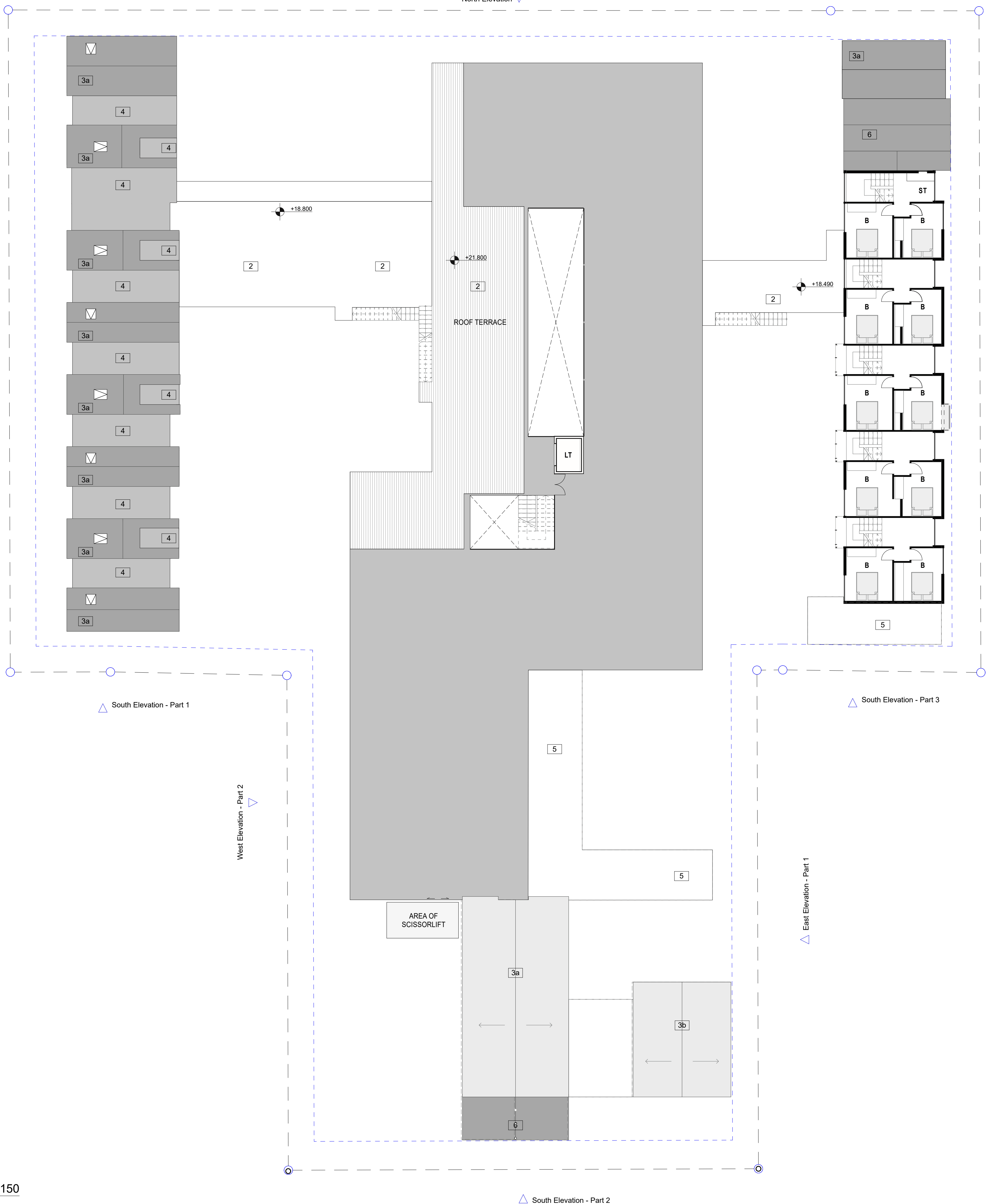
Published for
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North Elevation ▾

West Elevation - Part 1 ▾



East Elevation ▾

South Elevation - Part 1 ▾

South Elevation - Part 3 ▾

West Elevation - Part 2 ▾

East Elevation - Part 1 ▾

South Elevation - Part 2 ▾

PUBLIC FLOOR AREA - SECOND FLOOR

- Zone of public floor area
- Residential Units
Nil
- On site managers unit
Nil
- Residential Activity
Nil
- Food and Beverage (cafe)
Nil
- Entertainment / Event space
Nil
- Health Care / Counselling
Nil
- Support Services
Nil
- Basketball Court
Nil

NOTE:

This drawing is intended to be read in colour



RevID	CHD	Change Name	Date
01			24/02/2020

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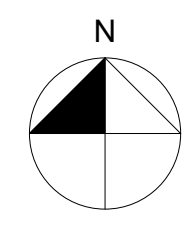
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Public Area - Second Floor

Scale
1:150 @ A1
Drawn
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22/05/2020
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Project no.	Drawing no.	Rev.
18-008	A906	01





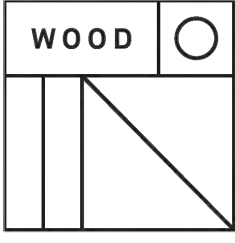
Appendix 3

Landscape Plans

Youth Hub

Landscape plans

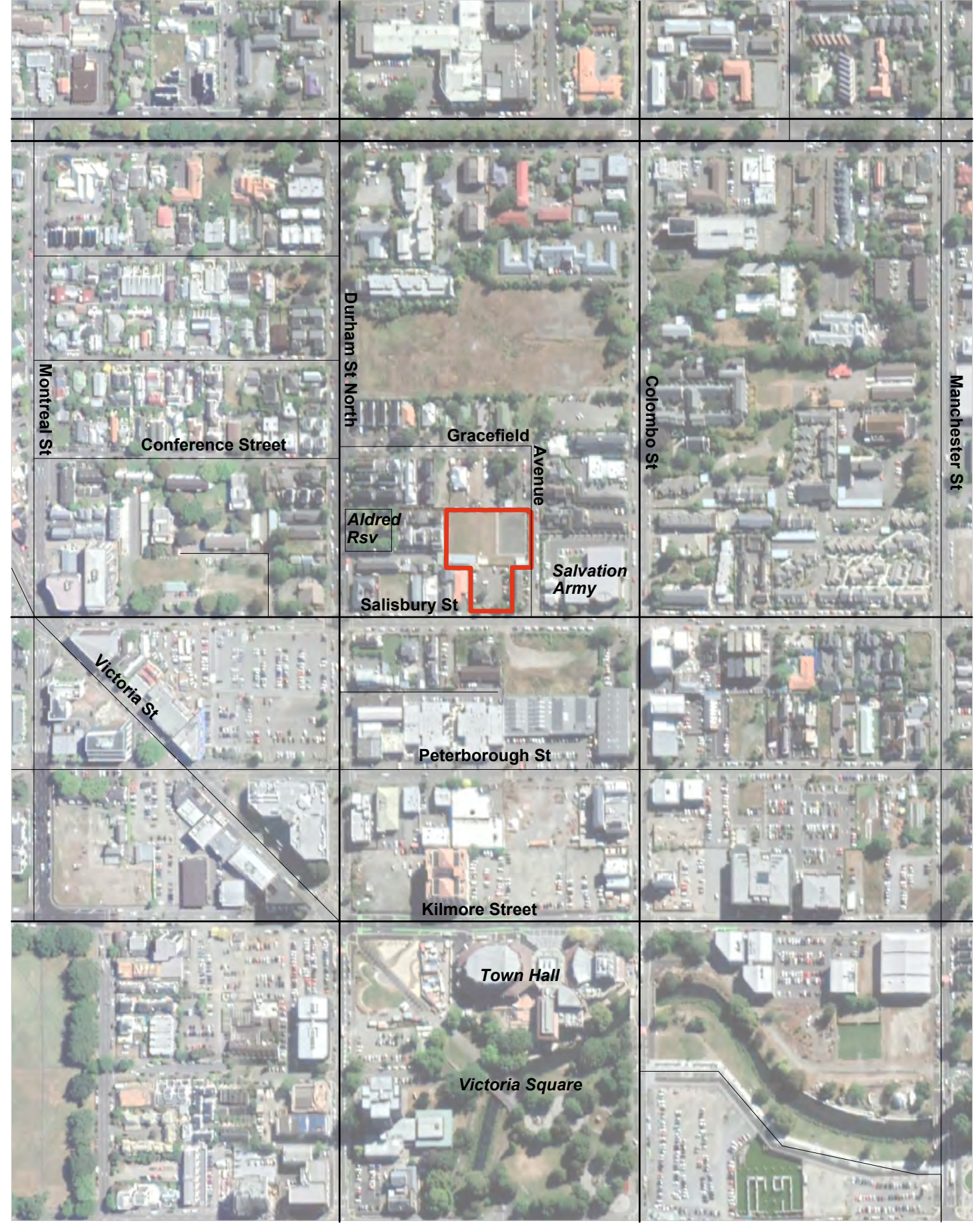
The Youth Hub Trust Te Hurihanga o Rangatahi
 109 Salisbury Street, Christchurch



Stage/Job # **J0086.17 - YouthHub**
 Issue **Consent Issue**
 Revision No. **7**

Contents

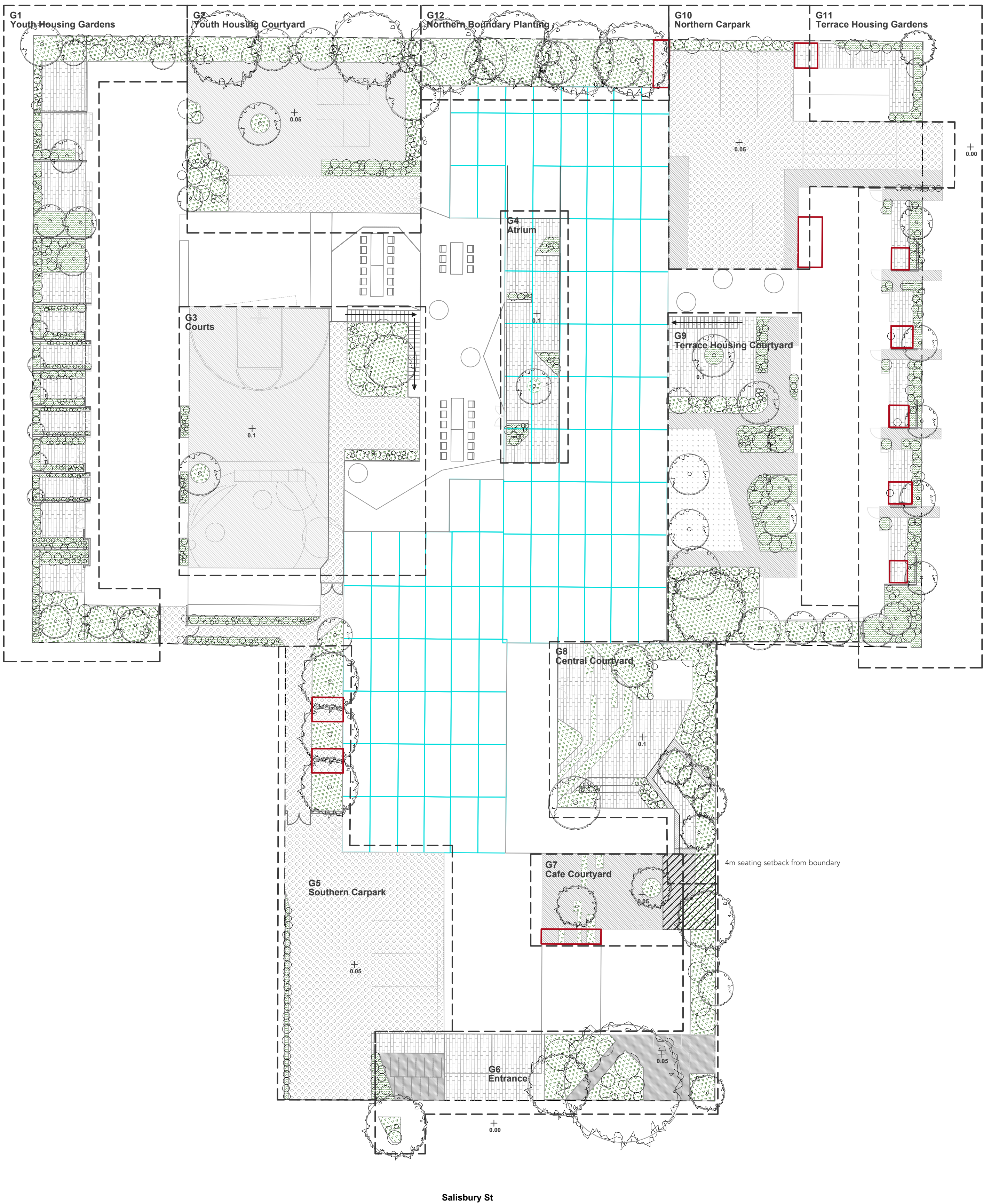
Page 1 -	Cover and Site location
Page 2 -	L102 Concept plan - Ground level
Page 3 -	L103 Concept plan - Terraces level
Page 4 -	Landscape concept plan details
Page 5 -	L201 South Elevation
Page 6 -	L202 East Elevation
Page 7 -	L203 North Elevation
Page 8 -	L204 West Elevation
Page 9 -	L04 Ground level Landscape Uses
Page 10 -	L05 Upper level Landscape Uses
Page 11 -	L06 Combined Landscape Uses



L101 Site location plan

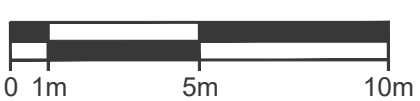
1:4000 @ a2





Not for construction purposes - consent issue

L101 | Concept Plan
Ground level



Youth Hub
Landscape plans

The Youth Hub Trust
Te Hurihanga o Rangatahi

Stage/Job # **J0086.17 - YouthHub**
Issue **Consent Issue**
Revision No. **7**

Drawing Date 5/25/2020
Scale 1 : 200
Page format A2
Drawn by Steffan Kraberger

Legend

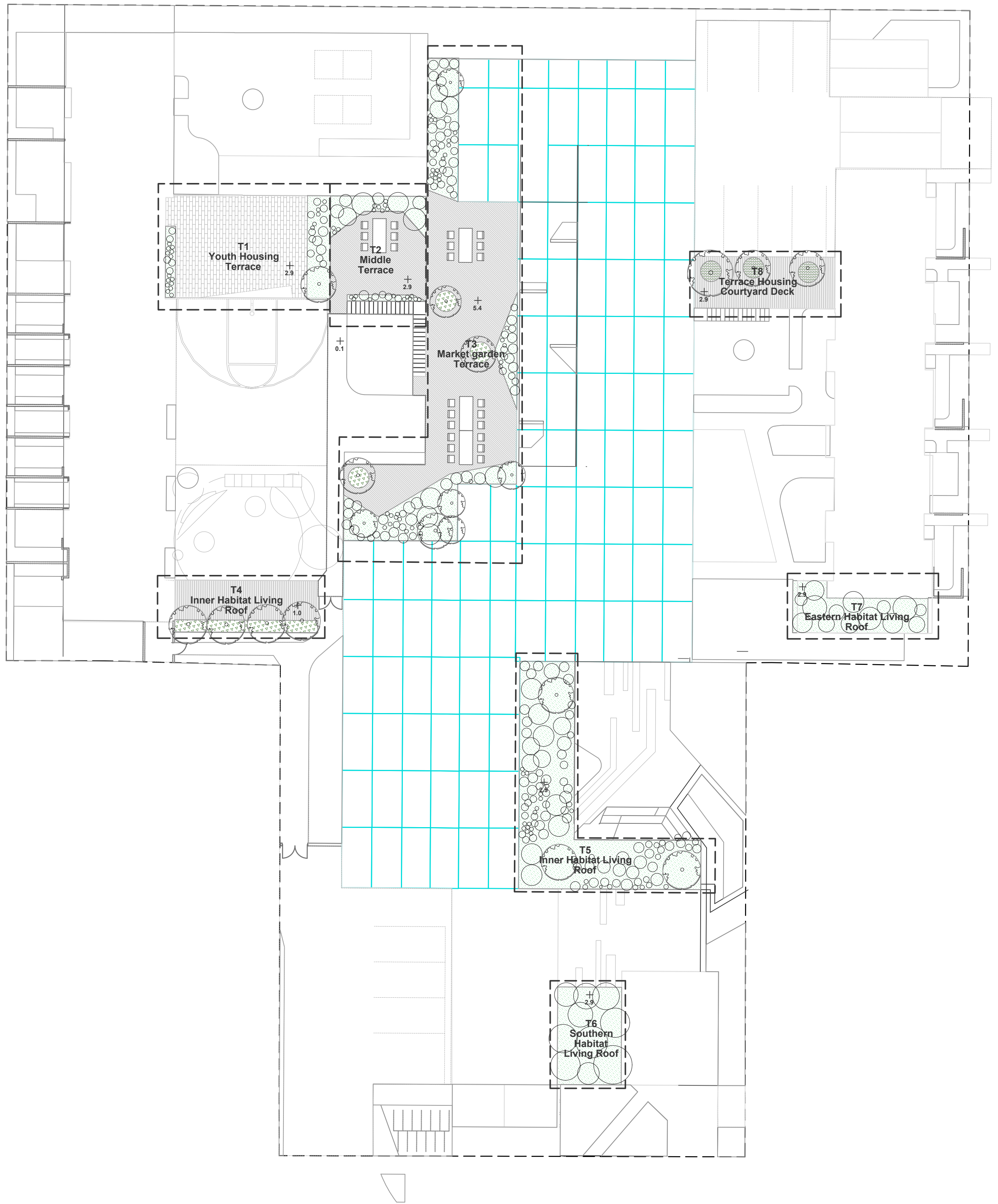
- Low native shrubs
- Medium native trees/shrubs
- Mixed exotic/native shrub garden
- Market/Vegetable Garden
- Permeable paving with groundcover
- Grass
- Native living roof (invertebrate habitat)
- Concrete
- Paving
- Decking
- Waste bin storage areas

WOOD



Gracefield Avenue

Salisbury St



Not for construction purposes - consent issue

L102 | Concept Plan
Upper levels

Youth Hub
Landscape plans

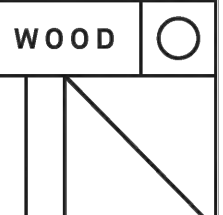
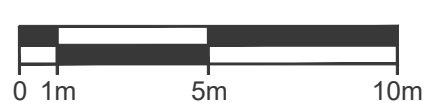
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Te Hurihanga ō Rangatahi

Stage/Job # **J0086.17 - YouthHub**
Issue **Consent Issue**
Revision No. **7**

Drawing Date 5/25/2020
Scale 1 : 200
Page format A2
Drawn by Steffan Kraberger

Legend

- | | | | |
|--|-----------------------------------|--|---|
| | Low native shrubs | | Native living roof (invertebrate habitat) |
| | Medium native trees/shrubs | | Concrete |
| | Mixed exotic/native shrub garden | | Paving |
| | Market/Vegetable Garden | | Decking |
| | Permeable paving with groundcover | | Grass |



Landscape concept plan details

G1 Youth Housing Private Gardens

- 1.6m brick nib walls lead out to boundary at the intersection of each adjoining flat, dividing the private gardens.
- Planting within this framework is anchored by a series of small deciduous trees, including edible Fig, Kouosa dogwood, Louois Van Houtte Elm, and Crab apple.
- Underplanted with low care native evergreen shrubs, particularly on north side of courtyard (in shade). Sunny south side of courtyard planted with hardy berry shrubs such as NZ cranberry and blueberries.

G2 Youth Housing Courtyard & G12

- Slated screen fence along boundary, with dense native shrub plantings behind, using predominantly large (2-4m) small leaved shrubs.
- The two trees directly north of the courtyard are deciduous ash to retain light through winter, with deciduous fruit trees in the center gardens of the courtyard. Large evergreen black beech *Fuscospora solandri* structure the northeast corner and continue along the northern boundary of the main building as shown on the plan as G12.

G3 Courts

- Large non-covered multi-sport space, surfaced using a variety of colored concretes with some wet-pour areas.
- Surrounding pathways and plantings tie the broad open area into the building context, with soft surfacing and a number of leafy green gardens, focusing on a close combination of *Coprosma repens*, *Coprosma lucida*, *Griselinia littoralis*, and *Macropiper excelsum* in shadier areas.

G4 Atrium

- Intimate paved alleyway with low nib wall dividers to create seating and rest areas alongside the general thoroughfare.
- Use of effective indoor exotics and natives, including *Fuschia procumbens* and *Meryta sinclairii*.
- Elevated walkways above to allow alternative thoroughfare and breakout for upper tenancies.

G5 Southern Carpark

- Permeable paving using 'grass pavers' with creeping groundcovers throughout such as creeping thyme; or alternative permeable paving solution such as Firth Surepave permeable.
- The carpark will be framed with a narrow fenceline planting of climbing NZ Jasmine (*Parsonia heterophylla*), with *Fuschia procumbens* at the base. Other plantings along the south edge with the small triangular inset planter, and the existing street side planter with use a combination of low native shrubs including *Olearia nummularifolia*, *Teucrium parvifolium*, *Pseudowintera colorata*.

G6 Entrances

- Retention of large maple and street tree.
- Composed native shrub plantings separate carpark, main entrance and cafe entrance. Use of plants such as *Olearia nummularifolia*, *Teucrium parvifolium* etc.

G7 Cafe courtyard

- An intimate courtyard to service the cafe, with edible plantings and referencing the materials of the central courtyard.

G8 Inner Courtyard

- The inner courtyard is focused on creating a sense of sanctuary.
- A series of umbrella-form trees (lifted canopies) creating a feeling of soft enclosure, further framed with planter beds of swathed native shrubs ramping up the walls of the courtyard.
- Native groundcover bands set into the pavement further the grounding of the pavement.
- A sunken courtyard to the east is framed under 2-3 umbrella canopy trees, creating a subtle gateway into the sanctuary area.

G9 Terrace Housing Courtyard

- A series personal small gardens line the east wall against the housing with hardy berry shrubs such as fejoa, NZ cranberry, blueberries, and *Fuschia exortica*.
- A feature *Metrosideros umbellata* (southern rata) sits against the west boundary (central) building, while the large scale of a *Fuscospora solandri* (black beech) in the south-west corner masks the scale of the central building, as well as enclosing the southern edge courtyard.
- A vegetable garden services the terrace housing, centered with two large prunus (a pear and an apple tree).

G10 Northern carpark

- Permeable paving using 'grass pavers' with creeping groundcovers throughout such as creeping thyme; or alternative permeable paving solution such as Firth Surepave permeable.
- Carpark framed with a significant body of lush hardy evergreen natives such as *Olearia* spp., *Psuedopanax laetum*, or *Dodonaea viscosa*.
- Permeable paving in Northern carpark and out through North East gated entry, with surface treatment change to exposed aggregate pathway through entrance archway from Gracefield Avenue.
- Exposed aggregate pathway continues across to entrance, and ties into bike sheds on the east and west sides of the carpark.

G11 Terrace Housing gardens

- Brick nib walls lead out to boundary at the intersection of each adjoining flat.
- Planting within this framework is anchored by a series of feature trees (e.g. *Olearia paniculata* (Akiraho), *Sophora microphylla* (Kowhai) or *Carpodetus serratus* (Putaputaweta). Classical exotic shrubs such as viburnum and azalea's ramp down between these frames to accentuate the terrace house intervals.
- Enclosed courtyard with edge planting and brick wall at North/East Corner, servicing caretaker house.

G12 Northern boundary of main block

- Several large trees screen the main block on the northern boundary embodying the older plain ecosystem of the site. All evergreen natives, these will consist of *Prumnopitys ferruginea* (Miro), *Carpodetus serratus* (Putaputaweta), and *Elaeocarpus hookerianus* (Pokaka).

T1 Youth Housing Terrace

- An intimate terrace servicing the private courtyard and residence. Raised living roof garden bed edges on the eastern side provide privacy, while tables and washing line on the west provide function. A movie projector is setup against the south wall.

T2 and T3 Market Garden Terraces

- A close combination of habitat living roofs with roof top seating and socializing areas set amongst the glass houses

T4 Court terrace

- Positioned above the courts, this terrace works as a non-invasive viewing deck. The southern edge of the terrace hosts a row of large evergreen trees e.g. *Carpodetus serratus* (Putaputaweta).

T5 Native habitat living roof

- Livingroof build-up system with planting and structures to create and protect a gecko and invertebrates habitat.

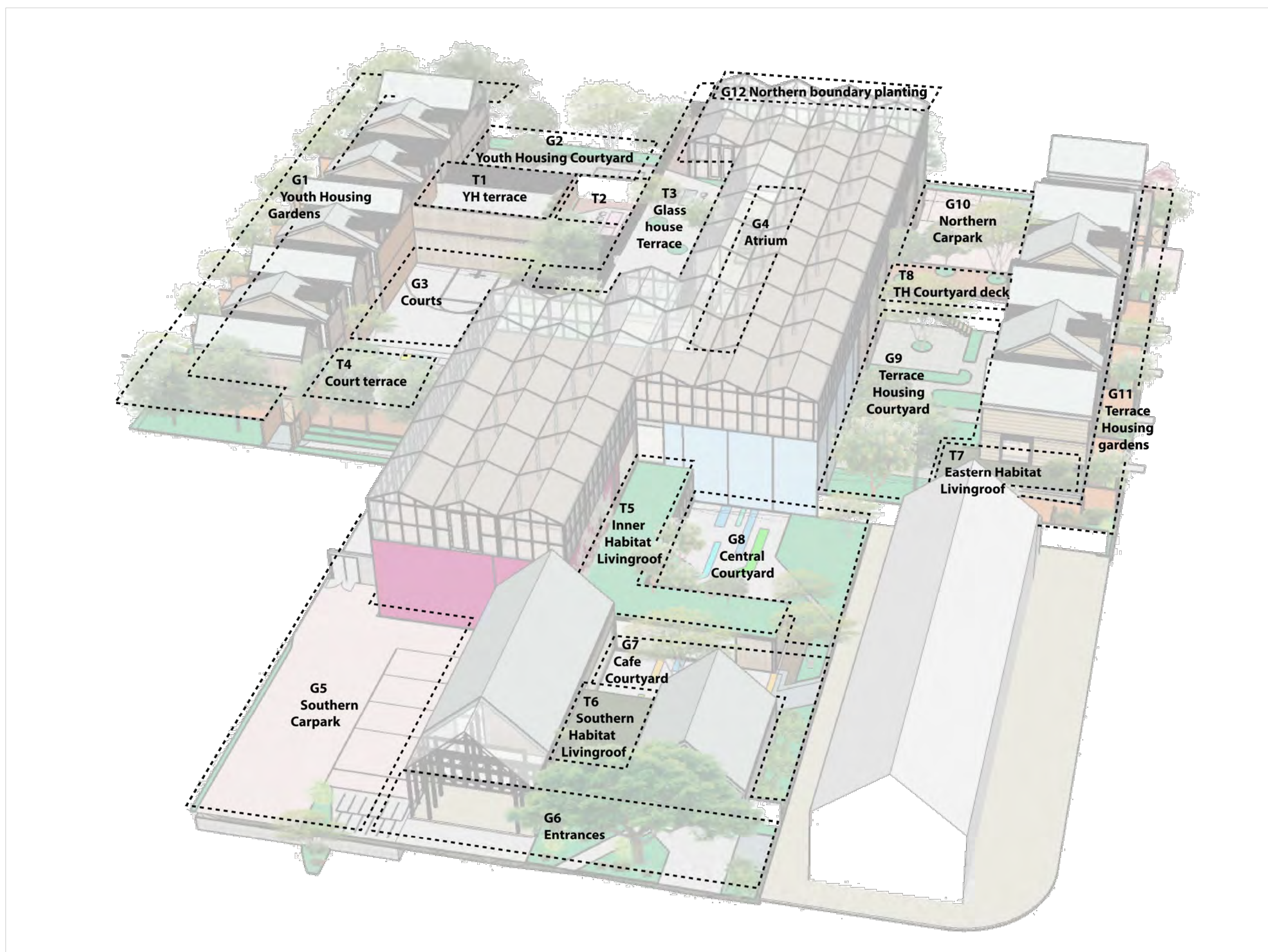
T6 and T7 Native habitat living roof (Large plantings)

- Testing the capacity of the livingroof build-up system to encourage large native plantings as a feature for the entrances, while creating habitat.

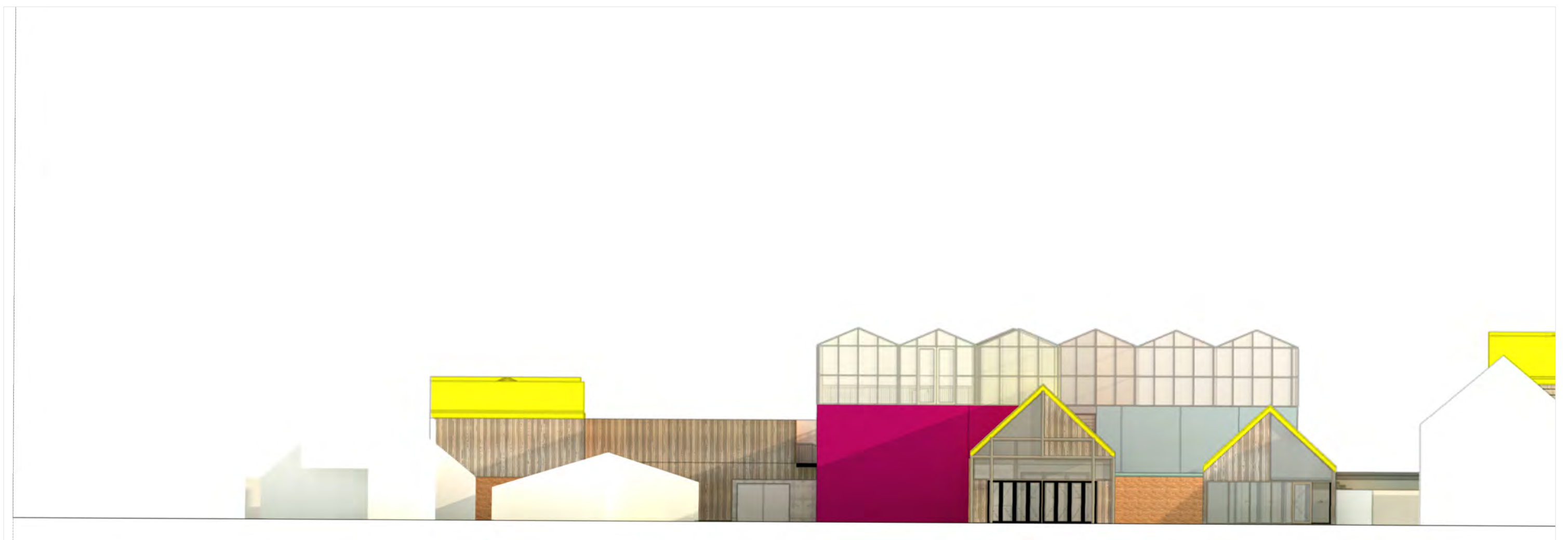
T8 Terrace Housing terrace

- Small social dwelling space, extending on Terrace Housing Courtyard while creating a separate sense of space. Focuses on a sense sanctuary 'up among the tree tops'.

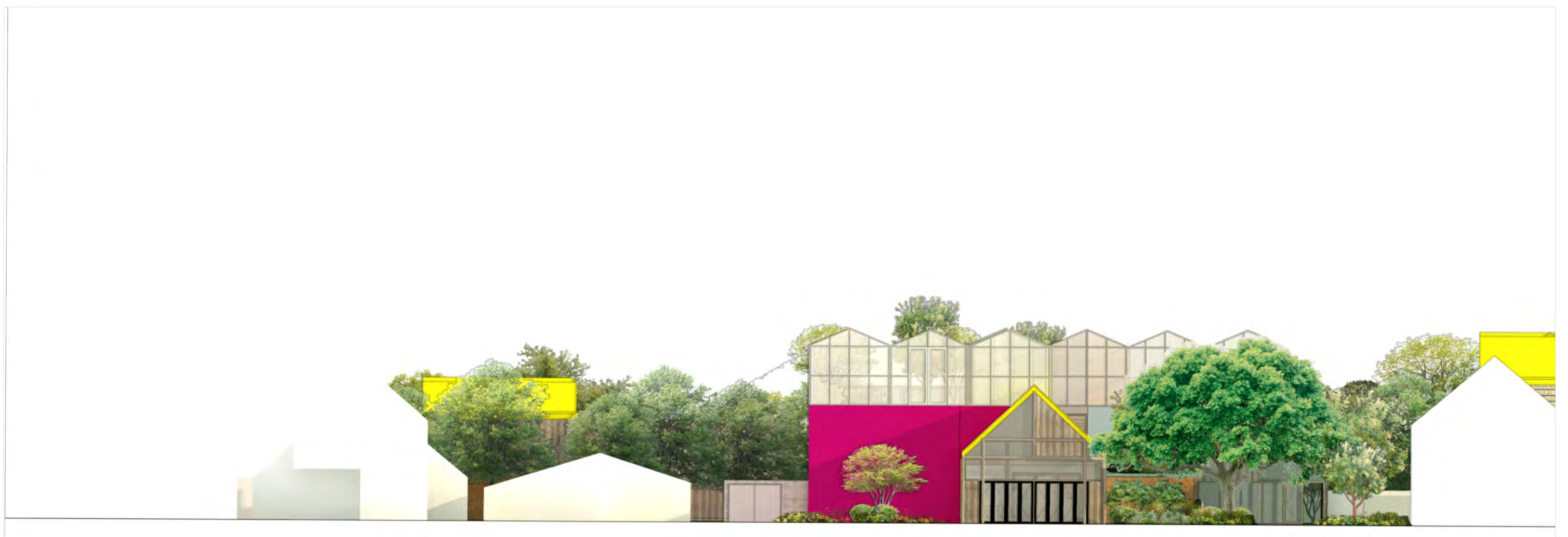
- Note all native trees (approximately 35+) as shown on concept plans must not be less than 1.5 metres high at the time of planting



L501 Concept zones diagram

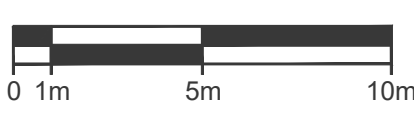

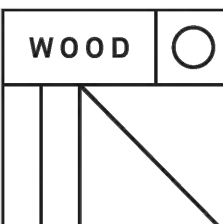


L201.1 South Elevation - Proposed Buildings



L201.2 South Elevation - Proposed Buildings & Landscape

Not for construction purposes - consent issue

<p>L201 South Elevation Salisbury St</p>  	<p>Youth Hub Landscape plans</p> <hr/> <p>The Youth Hub Trust Te Hurihanga ō Rangatahi</p>	<p>Stage/Job # J0086.17 - YouthHub Issue Consent Issue Revision No. 7</p> <hr/> <p>Drawing Date 5/25/2020 Scale 1 : 200 Page format A2 Drawn by Steffan Kraberger</p>	<p>Legend</p> 
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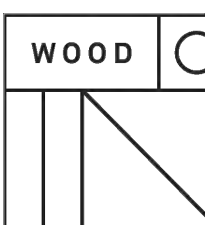


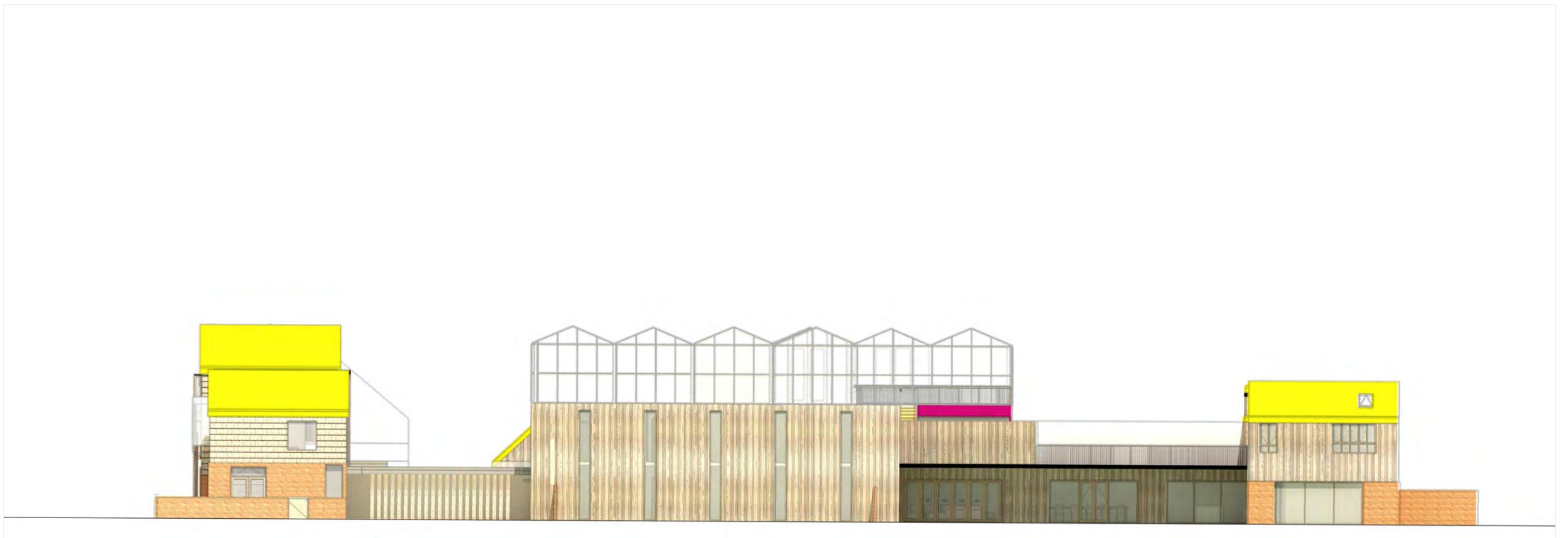
L202.1 East Elevation - Buildings only



L202.2 East Elevation - Landscape and Buildings

Not for construction purposes - consent issue

<p>L202 East Elevation Gracefield Av</p>  	<p>Youth Hub Landscape plans</p> <hr/> <p>The Youth Hub Trust Te Hurihanga o Rangatahi</p>	<p>Stage/Job # J0086.17 - YouthHub Issue Consent Issue Revision No. 7</p> <hr/> <p>Drawing Date 5/25/2020 Scale 1 : 200 Page format A2 Drawn by Steffan Kraberger</p>	<p>Legend</p> 
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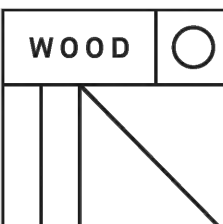


L203.1 North Elevation - Buildings only



L203.2 North Elevation - Landscape and Buildings

Not for construction purposes - consent issue

<p>L203 North Elevation Gracefield Av Nrth</p> 	<p>Youth Hub Landscape plans</p> <hr/> <p>The Youth Hub Trust Te Hurihanga o Rangatahi</p>	<p>Stage/Job # J0086.17 - YouthHub Issue Consent Issue Revision No. 7</p> <hr/> <p>Drawing Date 5/25/2020 Scale 1 : 200 Page format A2 Drawn by Steffan Kraberger</p>	<p>Legend</p> 
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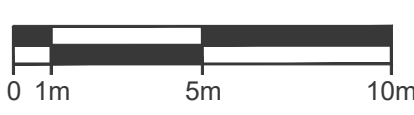

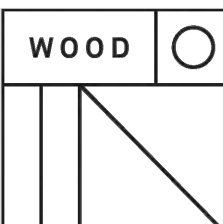


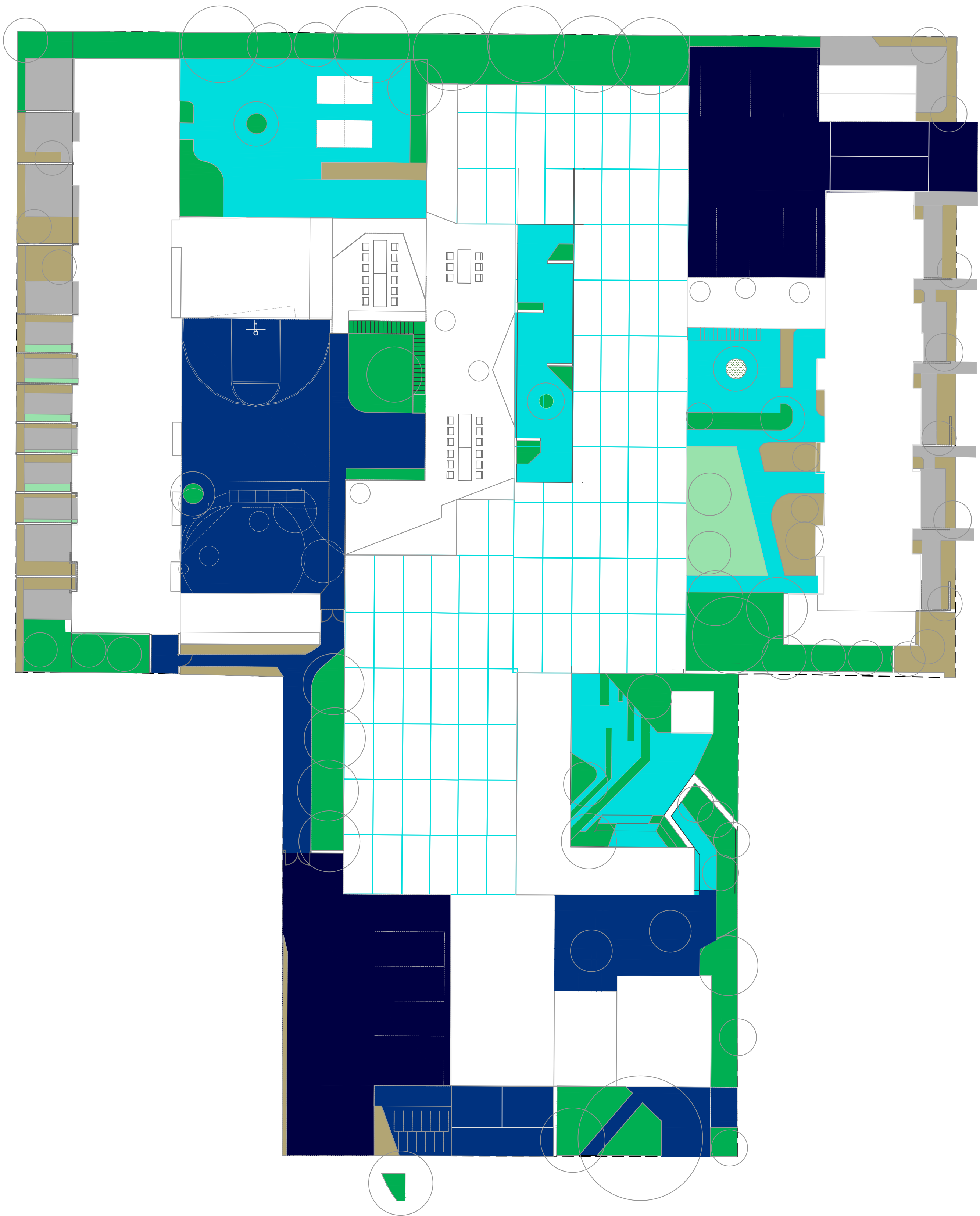
L204.1 West Elevation - Buildings only



L204.2 West Elevation - Landscape and Buildings

Not for construction purposes - consent issue

<p>L204 West Elevation Internal</p>  	<p>Youth Hub Landscape plans</p> <hr/> <p>The Youth Hub Trust Te Hurihanga ō Rangatahi</p>	<p>Stage/Job # J0086.17 - YouthHub Issue Consent Issue Revision No. 7</p> <hr/> <p>Drawing Date 5/25/2020 Scale 1 : 200 Page format A2 Drawn by Steffan Kraberger</p>	<p>Legend</p> 
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Not for construction purposes - consent issue

L104 | Landscape Uses Plan
Ground level

Youth Hub
Landscape plans

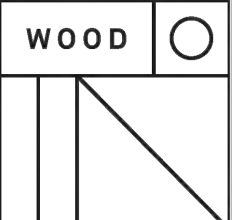
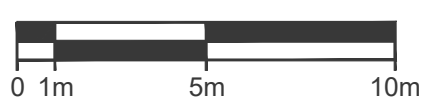
The Youth Hub Trust
Te Hurihanga ō Rangatahi

Stage/Job # **J0086.17 - YouthHub**
Issue **Consent Issue**
Revision No. **7**

Drawing Date 5/25/2020
Scale 1 : 200
Page format A2
Drawn by Steffan Kraberger

Legend

- Private Courtyard (Hardscape) - 165m²
- Communal spaces (Hardscape) - 414m²
- Public Spaces (Hardscape) - 410m²
- Permeable paver carpark - 429m²
- Native garden, trees & habitats - 463m²
- Mixed exotic/native gardens - 182m²
- Orchard/Garden - 43m²





Not for construction purposes - consent issue

L105 | Landscape uses plan
Upper levels

Youth Hub
Landscape plans

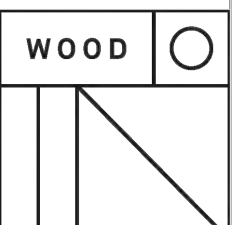
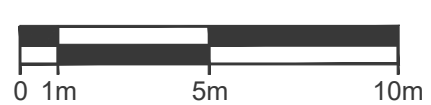
The Youth Hub Trust
Te Hurihanga ō Rangatahi

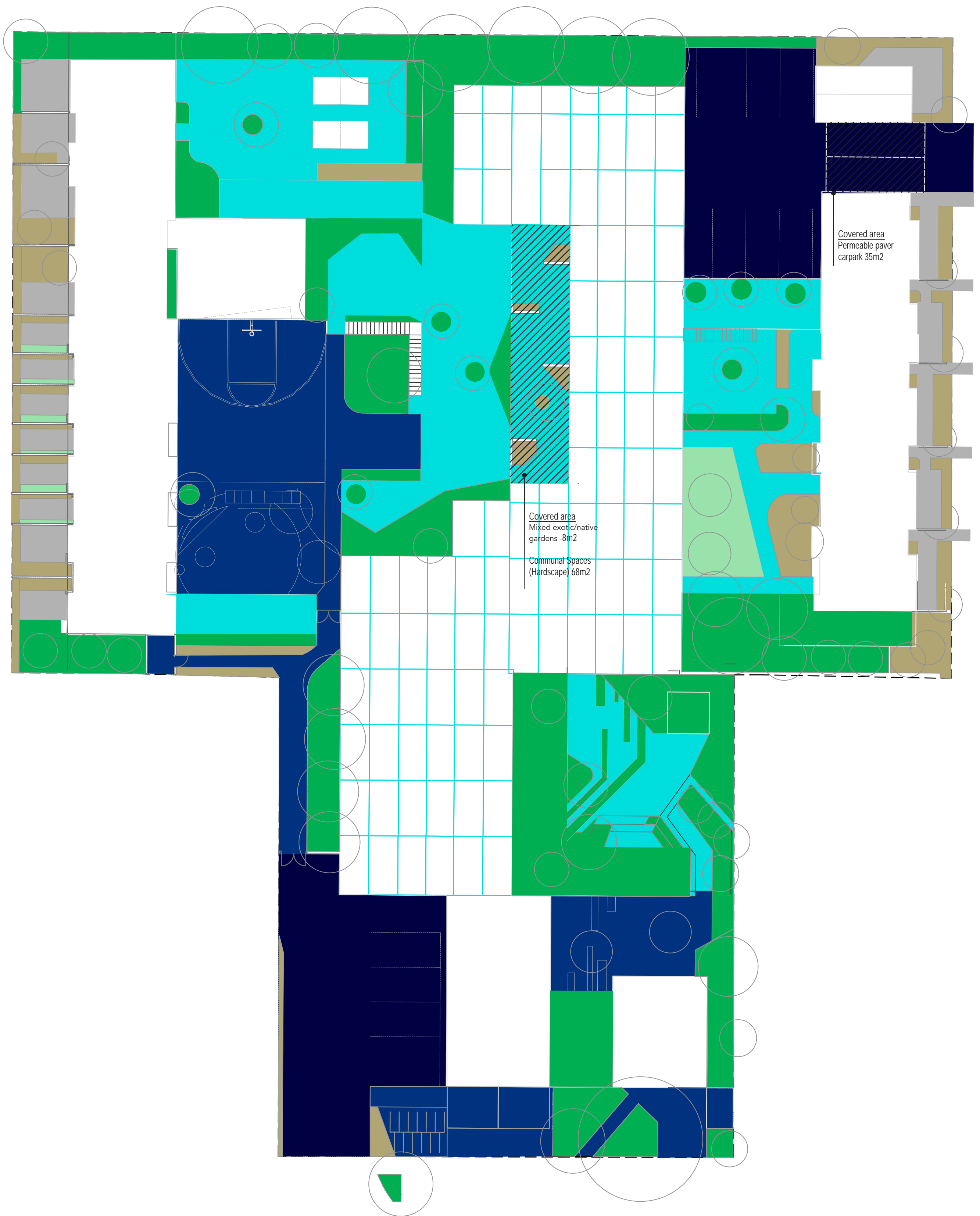
Stage/Job # **J0086.17 - YouthHub**
Issue **Consent Issue**
Revision No. **7**

Drawing Date 5/25/2020
Scale 1 : 200
Page format A2
Drawn by Steffan Kraberger

Legend

- Communal spaces (Hardscape) - 301m²
- Native garden, trees & habitats - 231m²





Not for construction purposes - consent issue

L106 | Landscape Uses Plan
Combined

Youth Hub
Landscape plans

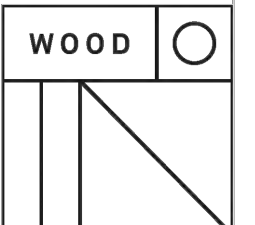
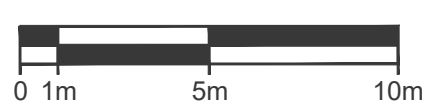
The Youth Hub Trust
Te Hurihanga ō Rangatahi

Stage/Job # **J0086.17 - YouthHub**
Issue **Consent Issue**
Revision No. **7**

Drawing Date 5/25/2020
Scale 1 : 200
Page format A2
Drawn by Steffan Kraberger

Legend

- Private Courtyard (Hardscape) - 160m²
- Native garden, trees & habitats - 689m²
- Communal spaces (Hardscape) - 715m²
- Mixed exotic/native gardens - 187m²
- Public Spaces (Hardscape) - 415m²
- Orchard/Garden - 43m²
- Permeable paver carpark - 429m²





Appendix 4

Operational Statement



The Youth Hub Christchurch Te Hurihanga o Rangatahi

Operational Statement by Dr Sue Bagshaw Chairperson of the Youth Hub Trust, February 2020.

The Youth Hub Trust was set up in 2017 to provide housing and buildings for organisations which work with young people, to work from. It can be seen as a supported living village for young people.

The Reason for the Youth Hub

The reason for the Hub is to ensure that young people have access to the support they need for the development of their full potential.

The stage of life between puberty and the period when the structural development of the brain has finished is very important and nurturing is needed as much as in the first five years of life. The age at which this development happens is highly variable and can be from 10 to approximately 25 years old. It takes at least ten years to complete. The Trust has purposely brought together those organisations that can aid becoming a successful adult, especially for those who have not been able to access that help from elsewhere. Young people need education, training, employment, support from caring adults, secure income, and a home. They may need health and legal advice and they need to learn the skills for independent living. Around 80% of young people receive this support from their families. However, there are about 20% who for many different reasons are not so fortunate.

Often services that provide support are spread out in different parts of the city. This means that they are inaccessible to a person with little money, no transport and no knowledge of who to go to or how to get there. Thus the Trust is bringing together all those providers of health care, housing, catch up education, employment and training opportunities, and other supports to make them easy to use for young people who would otherwise go without. The majority of the facilities will be for 10 to 25 year olds but the housing will be for those between 16 and 25 years. See Appendix One for the list of organisations that are meeting together now and plan to take part.

The most important part of helping young people develop well is a safe, nurturing place to live. Safe, stable housing underpins the ability to keep a job and therefore gain income, identity, and the connection to others in the housing contributes to wellbeing. The point of the wrap around services, that can be easily accessed from the housing, is to treat any underlying illness or disability that has led to that young person becoming ill or unemployed or not attending education, and providing opportunities to rectify those issues.

Unique

This Hub of support for young people is unique in Aotearoa New Zealand. Christchurch is known to be innovative in how it cares for young people, and has led the way in developing new ways of assisting the development of good mental health. This project is believed to be at the forefront

globally of integrated care and development for young people. There are examples of Youth One Stop Shops all over the country with one organisation providing many services, but none also provide housing, or bringing together many different organisations. The housing model is based on a model called Foyer in Europe and the United Kingdom and Covenant House in North America. These models provide housing for young people but very limited onsite support. Their principle of only accepting young people into the housing who are in training or looking for employment will be adopted in our model. The housing is important but, just like in any home, support needs to be there as well. Recreation and creative activities that build skills and meaning to life are as important as training and employment. Thus these will also be provided.

The Site

As a supported living village, focusing on assisting the development of young people to become vital citizens of the future, the site of the project is crucially important. It needs to be near a central point for transport, so that it is accessible to young people from every part of the city. It will bring young people into the city who can bring with them contributions of creativity, and energy, and be useful employees, as they learn those skills. As they are developing, and being integrated into adult society, they need an environment that supports them feeling that they are part of the city and that they have a place where they belong. The residential part of the central city is growing and the Trust feels that this will be an important part of including young people in this movement of making Christchurch a “liveable city”. The site on Salisbury street is 15-20 minutes’ walk from the Bus Exchange, within walking distance of the resources at Turanga, Margaret Mahy playground, and the new Metro Sports facility. There is no previous housing on the site, so this project will make a big contribution to the amount of housing available.

Connection – ie a sense of belonging, being accepted by and being a part of a community, or family or school is vitally important to development, as evidenced by research done nationally and internationally. The importance of being a part of a residential community cannot be overemphasised. It is not only important in the sense of connection but also important for a process of transition to becoming an adult taking a responsible part in the life of a community. We hope that the young people who are resident at the Hub might be able to communicate with and get to know adults in the area, so that they learn how to be a good neighbour. This could not happen in an industrial or commercial environment. Hence the necessity of siting the Hub in a residential area close to the City, containing housing that is affordable and accessible for young people.

The Value

The importance of this unique contribution to the life of our city is not just in what it can do but also in how it will be able to reach out into the suburbs. It can be interrelated with other organisations for young people to provide support and training to them, and can create more opportunities for the healthy development of young people. As our population ages we need every young person to be able to contribute. We can’t afford for any of them to slip behind, and every one of them needs to be able to have equal opportunities to fulfil their potential. We have too many children and young people in the care of the State who don’t get those opportunities. Too many young people who end their lives. There are many organisations trying to help, bringing them together will make them more effective in providing solutions to the problems that developing children and young people face. The

Hub is planning to have outreach to satellite services in other parts of the city in the long term so there is no intention to further expand on the central city site.

The Management Plan

The housing is being planned so that there are two phases of support. The first level will be in single ensuite rooms in one building with shared kitchen, lounge and laundry. There will be youth workers living onsite to teach living skills and to ensure behavioural standards of respect are maintained. No alcohol or other drugs will be allowed on the premises. Here young people can learn the skills of budgeting, shopping, cooking and cleaning. They will pay income related rent and will be taught how to find their own accommodation as well as employment. They will be able to stay from 3-18 months.

The second phase will be about learning how to live with other people. This will be in the three bedroom apartments. There will be no live-in support but there will be daily youth worker support and also the involvement of all the organisations present in the Hub. The same principle of finding their own employment and housing will apply. We will work towards providing future landlords with good tenants in the next phase of independent living as adults. We will work with private landlords and other housing providers to ensure opportunities for young people to move out.

The Youth Health Centre, which will be an important part of the Hub of organisations, has been operating as a one stop shop since 1995 and has acquired much valuable experience in helping young people. In all the time that it has been operating there have never been any incidents of violence and only one accidentally broken window. Youth workers are particularly trained in behaviour management as part of supporting development. The Youth Health Centre will be in charge of triaging admissions to the housing on the basis of need and suitability. The Christchurch Methodist Mission provides 'Housing First' projects and has many years of experience in handling tenancy agreements and managing rental agreements for less fortunate families and young people.

The Hub organisations will generally run services from 9am until 5.00pm. The evening activities will all finish at 10.00pm. There will be an expectation of low levels of noise and activity in the housing from 10pm. If young people want to enjoy the company of a larger group of friends, they will be able to use some of the wider facilities for parties but will be asked not to use the accommodation part.

There will be an onsite caretaker and facilities manager who will provide security for the site night and day.

Staff of organisations and residents will be encouraged to travel by bus, bike and scooter. There will be covered bike and scooter parks and some car parking on the site. An agreement has been reached so that car parking can also be arranged, especially for staff, at the new Casino Hotel that is being planned further down Salisbury street. As Salisbury Street is being developed for two-way traffic, this will make it easier for the use of parking away from the residential area.

Services for the Community

The Hub will run a café as a training and first job opportunity, but this will be available for local residents to use. Music and drama performances by young people will also be available for residents of the city to enjoy. The market garden will be producing high quality organic vegetables which will be available for sale on the doorstep as well as at the Riverside Market. Cultivate, the organisation

running the market garden, collects compost from surrounding cafes and businesses and uses the principles of re-use, re-cycle and re-generate in its operations. The whole Hub will be built for zero energy requirements and sound proofing will be an important part of the engineering requirements.

Appendix One

The following organisations are involved and committed to moving in to the Youth Hub Christchurch:

298 Youth Health run by the Korowai Youth Well-Being Trust- the whole organization will be based at the Hub and will contribute primary health services including physical, and mental health, and social services.

Collaborative Trust – Provision and maintenance of a training area with 50 seat capacity which can also be divided up for smaller meetings for Hub organizations and others to use. The large space to be shared for recreational and entertainment purposes with White Elephant. They will maintain an office space that can be shared with Te Kura and be used as a library for the whole facility.

Te Kura – Correspondence School class room for young people doing education on line with a tutor available and as a home work quiet study space.

Youthline –Telephone support, including after business hours; the whole organization will be based at the Hub, and they will run a cafe

Cultivate - will have a youth worker presence and assist with employment, training and a café, and utilise urban farming activities.

Catapult Employment- the whole organization will be based at the Hub assisting young people into work

Methodist Mission – the landlord for the housing and will handle rents and enquiries

Nurse Maude school health nurses will have an office, from which to provide health services in schools

Presbyterian Support Youth Team – will have a hot desk presence to provide income support and access to training and further education

Supporting Families with Mental Illness – will have a hot desk presence, but may move the whole organisation to the Hub.

Christchurch City Mission – providing youth alcohol and other drug counsellors.

Community Law – providing legal advice

VOYCE – supporting youth who have been in State Care

Qtopia – providing support for LGBTIA+ young people

Discussions are happening with Youthtown to be the organisation that provides development opportunities for young people in recreation, and the arts

Government Organisations including WINZ, Probation, Oranga Tamariki, Housing NZ, Careers NZ



Appendix 5

Compliance Assessment

Natural Hazards (Chapter 5)		
Rules – Natural Hazards		Status
5.5.1 Permitted activities	<p>a. All activities in the Liquefaction Management Area are a permitted activity unless specified in Rules 5.5.2 or 5.5.3, or as otherwise specified elsewhere in the District Plan.</p> <p>Comment – <i>the proposal is a permitted activity in the Liquefaction Management Area.</i></p>	Permitted Activity
General Rules (Chapter 6)		
6.1 Noise		
6.1.5.1.1 P3	<p>In the Central City, any activity that generates noise and which is not exempt by Rule 6.1.4.2.</p> <p>Activity Specific Standards</p> <p>a. Any activity that generates noise shall meet the Noise limits in the Central City in Rule 6.1.5.2.2.</p> <p>Comment – <i>All activities are expected to comply with the specified noise limits, with the exception of vehicles utilising the Salisbury Street car park.</i></p>	Does not comply
6.1.5.1.3 RD1	<p>Any activity listed in Rule 6.1.5.1.1 P1 or P3 that exceeds the noise limits in the activity specific standards by 10 dB or less.</p> <p>Comment – <i>vehicles utilising the Salisbury Street car park will exceed the daytime LAeq noise limit at 103 Salisbury Street by 4dB.</i></p>	Restricted discretionary
6.1.5.2.2 Noise limits in the Central City	<p>a. In the Central City, any activity that generates noise shall meet the Noise standards in Table 2 below at any site receiving noise from that activity, as relevant to the Category of Precinct in which the site receiving the noise is located (as shown on the Central City Entertainment and Hospitality Precinct Overlay planning map).</p> <p>c. Category 3</p> <p>All activities - 07:00-23:00 – 55 LAEq, 85 LAMax</p> <p>All activities – 23:00-07:00 – 45 LAEq, 75 LAMax.</p> <p>Comment – <i>vehicles utilising the Salisbury Street car park will exceed the daytime LAeq noise limit at 103 Salisbury Street by 4-8dB.</i></p>	Does not comply
6.1.6.1.1 P1	<p>Any activity listed in:</p> <p>...</p> <p>h. Rule 6.1.6.2.9 (Sensitive activities in the Central City) other than as provided for in Rule 6.1.6.1.4 D3.</p> <p>Activity specific standards:</p> <p>a. The activities shall meet the activity standards in the following rules:</p> <p>...</p> <p>viii. Rule 6.1.6.2.9 (Sensitive activities in the Central City),</p> <p>b. In the Central City, all activities shall also comply with Rule 6.1.5.</p> <p>Comment – <i>The proposal will meet the requirements of Rule 6.1.6.2.9 (Sensitive activities in the Central City).</i></p>	Complies
6.1.6.1.4 D3	<p>In the Central City, any residential activity or guest accommodation located within a Category 1 Precinct as shown on the Central City Entertainment and Hospitality Precinct Overlay planning map.</p> <p>Comment – <i>the site is not located within a Category 1 Precinct.</i></p>	N/A

6.1.6.2.9 Sensitive activities in the Central City	<p>a. Sensitive Activities in the Central City shall meet the following activity standards:</p> <p>i. Any sensitive activity shall achieve a minimum external to internal noise reduction of:</p> <ul style="list-style-type: none">A. Category 2 Precincts:<ul style="list-style-type: none">I. 35 dB Dtr, 2m, nT,w + Ctr for bedrooms;II. 30 dB Dtr, 2m, nT,w + Ctr for other habitable spaces.B. Category 3 Precincts adjoining the Category 1 Precinct:<ul style="list-style-type: none">I. 35 dB Dtr, 2m, nT,w + Ctr for bedrooms;II. 30 dB Dtr, 2m, nT,w + Ctr for other habitable spaces.C. Category 3 Precincts zoned residential, if within 75 metres of a Category 1 or 2 Precinct or H4 Stadium (Incorporating Spectator Events Facility) shown on Planning Maps 32 and 39, and not already covered by B. above:<ul style="list-style-type: none">I. 30 dB Dtr, 2m, nT,w + Ctr for bedrooms.D. Category 3 Precincts zoned Commercial Central City Business, Commercial Central City Mixed Use, Commercial Central City Mixed Use (South Frame) and Commercial Local and not already covered by B. above:<ul style="list-style-type: none">I. 30 dB Dtr, 2m, nT,w + Ctr for bedrooms. <p>Comment – the site is within a Category 3 Precinct but does not adjoin a Category 1 Precinct and is in excess of 75m from a Category 1 or 2 Precinct or H4 Stadium. The listed internal noise reductions are therefore not required and the proposal can otherwise comply with the rule.</p>	Complies
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6.1.7.1.1 P1	<p>Any activity listed in:</p> <ul style="list-style-type: none">a. Rule 6.1.7.2.1 (Sensitive activities near roads and railways); orb. Rule 6.1.7.2.2 (Activities near Christchurch Airport) <p>Activity Specific Standards</p> <ul style="list-style-type: none">a. The activities shall meet the activity standards in the following rules <ul style="list-style-type: none">i. Rule 6.1.7.2.1 (Sensitive activities near roads and railways); orii. Rule 6.1.7.2.2 (Activities near Christchurch Airport) <p>Comment – the proposal will comply with Rule 6.1.7.2.1.</p>	Complies
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6.1.7.2.1 Sensitive activities near roads and railways	<p>a. The following activity standards apply to new buildings, or alterations or additions to existing buildings, intended for a sensitive activity:</p> <ul style="list-style-type: none">i. External sound insulation - Any new building intended for a sensitive activity, and any alteration or addition to an existing building intended for a sensitive activity, located within 80 metres of the boundary of any state highway or railway designation, or within 20 metres of the edge of the nearest marked traffic lane of a collector road, or within 40 metres of the edge of the nearest marked traffic lane of a Main Distributor, Local Distributor or arterial road, shall either:<ul style="list-style-type: none">A. be designed and constructed to achieve a minimum external to internal noise reduction of 30 dB Dtr,2m,nT,w + Ctr to any habitable space; orB. be designed and constructed to meet with the following indoor design sound levels:<ul style="list-style-type: none">I. Rail noise inside bedrooms between 22:00 hours and 07:00 hours – 35 dB LAEq(1h);II. Rail noise inside habitable spaces excluding bedrooms – 40 dB LAEq(1h);III. Road traffic noise inside all habitable spaces – 40 dB LAEq (24hr); and	Complies
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IV. Rail and road traffic noise within any other building intended for a sensitive activity – maximum value recommended in AS/NZS2107:2000.

except where either:

V. the sound incident on the most exposed part of the outside of the building is less than 55 dB LAEq(1h) for rail noise or 57 dB LAEq(24h) for road traffic noise; or

VI. the nearest façade of the building is at least 50 metres from all state highway and railway designations and there is a solid building, fence, wall or landform that blocks the line-of-sight from all parts of all windows and doors to all parts of any state highway road surface or all points 3.8 metres above railway tracks.

ii. Compliance with Rule 6.1.7.2.1 can be achieved by either:

A. providing the Council with a design report (prior to construction) and a design certificate (prior to occupation) prepared by a suitably qualified acoustics specialist stating the design proposed is capable of meeting activity standard a.i.; and/or

B. conforming to the acceptable solutions listed in Appendix 6.11.4 Noise Attenuation Construction Requirements.

iii. For the purposes of ventilation systems, compliance with Rule 6.1.7.2.1 shall be confirmed by providing the product specifications; or a design certificate (prior to occupation) prepared by a suitably qualified acoustics specialist, stating the design proposed is capable of meeting the activity standards.

iv. Rail noise shall be deemed to be 70 LAEq(1h) at a distance of 12 metres from the edge of the track, and shall be deemed to reduce at a rate of 3 dB per doubling of distance up to 40 metres and 6 dB per doubling of distance beyond 40 metres;

v. Compliance with Rule 6.1.7.2.1 a.i.B. shall be confirmed by providing the Council with a design report prepared by a qualified acoustic engineer demonstrating compliance, prior to any sensitive activity or alteration occurring. The design shall take into account future permitted use of the collector roads and arterial roads, and railway and state highway designations outside the Central City, either by the addition of 2 dB to predicted sound levels or based on forecast traffic in 20 years' time.

vi. The indoor design sound levels in Rule 6.1.7.2.1 a.i.B shall be achieved at the same time as the ventilation requirements of the New Zealand Building Code. If windows are required to be closed to achieve the indoor design sound levels then an alternative means of ventilation shall be required within bedrooms.

vii. Where no traffic lane is marked, the distances stated shall be measured from 2 metres on the roadward side of the formed kerb. The classification of roads is shown in Appendix 7.5.12 Road Classification System.

viii. Ventilation systems where installed shall:

- A. generate sound levels not exceeding
 - I. 35 dB LAEq(30s) at night time in bedrooms; and
 - II. 40 dB LAEq(30s) in any other habitable space (excluding bedrooms) when measured 1 metre away from any grille or diffuser; and
- B. provide an adjustable airflow rate of up to at least 6 air changes per hour.

Comment – *Salisbury Street is classified as a Central City Local Distributor. Gracefield Avenue is a local road. The residential units and sheltered accommodation on site are approximately 44m (west block) and 43m (east block) from nearest marked traffic lane on Salisbury Street respectively.*

6.3 Outdoor Lighting

6.3.4.1 P1 Any activity involving artificial outdoor lighting, other than activities specified in Rule 6.3.4.5 NC1 or NC2. Complies

Activity Specific Standards:

- a. All fixed exterior lighting shall, as far as practicable, be aimed, adjusted and/or screened to direct lighting away from the windows of habitable spaces of sensitive activities, other than residential units located in industrial zones, so that the obtrusive effects of glare on occupants are minimised.
-

- b. Artificial outdoor lighting shall not result in a greater than 2.5 lux spill (horizontal or vertical) into any part of a major arterial road or minor arterial road or arterial route identified in Appendix 7.5.12 where this would cause driver distraction.

Comment – All outdoor lighting will comply with these rules.

6.3.5.1 P1	Any activity involving outdoor artificial lighting Activity Specific Standards: a. Any outdoor artificial lighting shall comply: i. with the light spill standards in Rule 6.3.6 as relevant to the zone in which it is located, and; ii. where the light from an activity spills onto another site in a zone with a more restrictive standard, the more restrictive standard shall apply to any light spill received at that site. Comment – The permitted lux spill specified in Rule 6.3.6 for Residential Zones (all other) is 4.0. On -site lighting will be designed to comply with this rule.	Complies
6.8 Signs	Comment – no specific signage is proposed at present. Any signage will be discreet and will either comply with the rules of the District Plan or will seek resource consent separately.	Complies

Transport (Chapter 7) See Integrated Traffic Assessment – Appendix 7

Subdivision, Development and Earthworks (Chapter 8)

8.9.2.1 P1 Earthworks	Earthworks: a. not for the purpose of the repair of land used for residential purposes and damaged by earthquakes; and b. if in the Industrial General Zone (North Belfast), greater than 20 metres from: [...] Activity Standard: a. Earthworks shall not exceed the volumes in Table 9 over any 12 month time period. [Table 9 states a maximum volume of 20m ³ /site] b. Earthworks in zones listed in Table 9 shall not exceed a maximum depth of 0.6m, other than in relation to farming activities, quarrying activities or permitted education activities. c. Earthworks shall not occur on land which has a gradient that is steeper than 1 in 6. d. Earthworks involving soil compaction methods which create vibration shall comply with DIN 4150 199902 and compliance shall be certified through a statement of professional opinion provided to the Council from a suitably qualified and experienced chartered or registered engineer. e. Earthworks involving mechanical or illuminating equipment shall not be undertaken outside the hours of 0700 – 1900 in a Residential Zone. f. Earthworks involving mechanical equipment, other than in residential zones, shall not occur outside the hours of 0700 and 2200 except where compliant with NZS6803:1999. g. Fill shall consist of clean fill. h. The activity standards listed in Rule 8.9.2.1 P3, P4 and P5. i. Earthworks shall not occur within 5 metres of a heritage item or above the volumes contained in Table 9 within a heritage setting listed in Appendix 9.3.7.2. j. In the Industrial General Zone (North Belfast): Activity Standards in Rule 8.6.14.	Does not comply
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Comment – Total volume of earthworks proposed, excluding those required for building foundations which will be subject to building consent and will not occur in advance of building consent, is 284m³ of excavation, with an equivalent amount of base course and hard surfacing replacement fill for ground level works, with a maximum depth of approximately 200mm. Roof top gardens are anticipated to require approximately 220m³ of fill to a depth of around 400mm.

Rule 8.9.2.3 RD1	Any activity listed in Rule 8.9.2.1 P1 or Rule 8.9.2.2 C1 that does not meet any one or more of the activity standards. Comment – the proposed earthworks do not meet rule 8.9.2.1 P1.	Restricted Discretionary
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Residential (Chapter 14)

Activity Status Tables – Residential Central City Zone		Status
14.6.1.1 P1 Residential activity	<p>a. No more than one heavy vehicle shall be stored on the site of the residential activity.</p> <p>b. Any motor vehicles and/or boats built, dismantled, repaired or stored on the site of the residential activity shall be owned by people who live on the same site.</p> <p>c. In relation to the building, dismantling, repair or storage of motor vehicles, the vehicles shall be contained in a building, or, if the vehicles are not contained in a building, there shall be no more than three vehicles involved.</p> <p>d. In relation to the building, dismantling, repair or storage of boats, collectively the boats shall occupy no more than 45m</p> <p>Comment- Six residential units and 22 bedrooms for sheltered living accommodation (plus two supervisors' bedrooms) are proposed.¹⁰ All units and sheltered living accommodation will comply with these Activity Specific Standards.</p>	Permitted
14.6.1.1 P5 Market gardens, community gardens, and garden allotments	Comment- A rooftop greenhouse market garden is proposed.	Permitted
14.6.1.1 P8 Any non-residential activity up to 40m ² gross floor area (including any area of outdoor storage) that is otherwise not provided for under Rule 14.13.2.1 P9 and P10	<p>a. Only those persons who reside permanently on the site can be employed in the activity.</p> <p>b. The maximum total number of hours the site shall be open to visitors, clients or deliveries for the activity shall be 40 hours per week, and shall be limited to between the hours of:</p> <p>i. 07:00 – 21:00 Monday to Friday, and</p> <p>ii. 08:00 – 19:00 Saturday, Sunday, and public holidays.</p> <p>c. The maximum number of vehicle movements per site, other than for residential activities, shall be:</p> <p>i. heavy vehicles: 2 per week; and</p> <p>ii. other vehicles: 16 per day.</p>	N/A

¹⁰ The District Plans defines Residential activity as 'means the use of land and/or buildings for the purpose of living accommodation. It includes: a residential unit, boarding house, student hostel or a family flat (including accessory buildings); emergency and refuge accommodation; and sheltered housing; but excludes: guest accommodation; the use of land and/or buildings for custodial and/or supervised living accommodation where the residents are detained on the site; and accommodation associated with a fire station. A Residential unit is defined as 'a self-contained building or unit (or group of buildings, including accessory buildings) used for a residential activity by one or more persons who form a single household'. The east wing units fall to be considered as residential activity within residential units. The west wing accommodation is residential activity (sheltered living) but will not be contained within a residential unit as the tenants will not form a single household.

d. Boarding animals on a site shall be limited to a maximum of four animals in the care of a registered veterinarian for medical or surgical purposes only.

e. Manufacturing, altering, repairing, dismantling or processing of any materials, goods or articles shall be carried out in a building.

Comment- *The proposed café will be approximately 154m² in area on the site, inclusive of kitchen, bathroom and outdoor courtyard area.*

<p>14.6.1.1 P9</p> <p>Any education facility, spiritual activity, health care facility, preschool (other than as provided for in Rule 14.6.1.1 P7), or guest accommodation up to 40m² gross floor area (including any area of outdoor storage used for activities other than residential activities), except those activities provided for in Rule 14.6.1.1 P10.</p>	<p>a. Only those persons who reside permanently on the site can be employed in the activity.</p> <p>b. The maximum total number of hours the site shall be open to visitors, clients or deliveries for the activity, other than for guest accommodation activities, shall be 40 hours per week, and shall be limited to between the hours of:</p> <ul style="list-style-type: none"> i. 07:00 – 21:00 Monday to Friday, and ii. 08:00 – 19:00 Saturday, Sunday, and public holidays. <p>c. The maximum number of vehicle movements per site, other than for residential activities, shall be:</p> <ul style="list-style-type: none"> i. heavy vehicles: 2 per week; and ii. other vehicles: 16 per day. <p>Comment- <i>N/A, the educational and health care aspects of the facility are part of the community facility.</i></p>	<p>N/A</p>
<p>14.6.1.1 P10</p> <p>Any community facility, preschool (other than as provided for in Rule 14.6.1.1 P7), or guest accommodation on Fitzgerald Avenue, or Bealey Avenue between Durham Street North and Madras Street.</p>	<p>a. The maximum total number of hours the site shall be open to visitors, clients or deliveries for the activity shall be 40 hours per week, and shall be limited to between the hours of:</p> <ul style="list-style-type: none"> i. 0:700 – 21:00 Monday to Friday, and ii. 08:00 – 19:00 Saturday, Sunday, and public holidays. iii. Except that these hours of operation in Rule 14.6.1.1 P10 a.i. and ai.ii. do not apply to guest accommodation. <p>b. The maximum number of vehicle movements per site per day for any activity, other than for residential activities, shall be 200 and:</p> <ul style="list-style-type: none"> i. Vehicles, other than heavy vehicles associated with any residential activity on the site, shall be included in determining the number of vehicle movements to and from any site. Vehicles parking on the street or on any other site, in order that their occupants can visit the site, shall also be included in determining the number of vehicles trips to and from any site. <p>Comment – <i>The site is not located on Fitzgerald Avenue, or Bealey Avenue between Durham Street North and Madras Street and the rule therefore does not apply.</i></p> <p><i>Nevertheless, it is noted that the youth oriented services, including the ancillary health centre, community organisations/services present on site, recreation/arts centre, events centre and associated facilities together comprise a community facility (all will relate to youth development and support services). The site collectively may be open to visitors in excess of 40 hours per week, within the hours specified in this rule. For example, most services will operate only 40 hours per week on weekdays, but the event and recreation aspects of the facility may occasionally open on evenings or weekends, extending the hours the site is open to visitors, to beyond 40 hours.</i></p>	<p>N/A</p>
<p>14.6.1.3 RD1</p>	<p>Any permitted activity or a cultural activity at 52 Rolleston Avenue Lot 2 DP496200), that does not meet one or more of the built form standards in Rule 14.6.2.</p> <p>Any application arising from the following built form standards shall not be limited or publicly notified:</p> <ul style="list-style-type: none"> Rule 14.6.2.3 Road boundary building setback Rule 14.6.2.5 Fencing and screening Rule 14.6.2.6 Tree and garden planting 	<p>Restricted Discretionary</p>

	<p>Rule 14.6.2.7 Minimum residential unit size</p> <p>Rule 14.6.2.8 Ground floor habitable space</p> <p>Rule 14.6.2.9 Outdoor living space</p> <p>Rule 14.6.2.10 Service space</p> <p>Rule 14.6.2.11 Minimum site density from development and redevelopment of residential units</p> <p>Comment- <i>the proposal will not comply with Rule 14.6.2.5 Fencing and screening, Rule 14.6.2.9 Outdoor living space, Rule 14.6.2.10 Service space, 14.6.2.6 Tree and garden planting and 14.6.2.11 Minimum site density from development and redevelopment of residential units</i></p>	
14.6.1.3 RD2	<p>a. Any activity involving the erection of new buildings and alterations or additions to existing buildings including all accessory buildings, fences and walls associated with that development, that result in:</p> <p>i. three or more residential units; or</p> <p>ii. one or two residential units on a site smaller than 300m² gross site area;</p> <p>b. Any application arising from this rule shall not be limited or publicly notified.</p> <p>Comment – <i>The proposal will result in six residential units.</i></p>	Restricted Discretionary
14.6.1.5 NC1	<p>Any non-residential activity not otherwise provided for as a permitted, restricted discretionary, discretionary or non-complying activity with a gross floor area over 40m² (including any area of outdoor storage used for that activity).</p> <p>Comments- <i>the proposed site provides for a non-residential activity (café) with a gross floor area over 40m² that are not otherwise provided for in the residential central city chapter of the Plan. The proposal also provides for a community facility that is not otherwise provided for.</i></p>	Non-Complying

Built Form Standards – Residential Central City Zone

14.6.2.1 Building height	<p>a. The maximum height of any buildings shall be as shown on the Central City Maximum Building Height Planning Map, except that the Central City Maximum Building Height Planning Map does not apply to the following land where a maximum building height of 20 metres shall apply to buildings for a retirement village:</p> <p>i. Lot 1 DP 77997 CT CB46D/74;</p> <p>ii. Town Section 118 DP 3780; and</p> <p>iii. Town Section 119 DP 3780.</p> <p>Comment – <i>The proposed development will be less than 14m in height.</i></p>	Complies
14.6.2.2. Daylight recession planes	<p>a. Buildings shall not project beyond a building envelope constructed by recession planes from points 2.3 metres above internal boundaries with other sites as shown in Appendix 14.16.2C, except that:</p> <p>i. Where an internal boundary of a site abuts an access lot, access strip, or access to a rear allotment, the recession plane may be constructed from points 2.3 metres above the furthest boundary of the access lot, access strip, or access to a rear allotment or any combination of these areas;</p> <p>ii. Where buildings on adjoining sites have a common wall along an internal boundary the recession planes shall not apply along that part of the boundary covered by such a wall.</p> <p>b. Where the building is located in a Flood Management Area, the exemptions in Rule 5.4.1.3 apply (for activities P1-P4 in Table 5.4.1.1b).</p> <p>c. The level of internal boundaries shall be measured from filled ground level except where the site on the other side of the internal boundary is at a lower level, then that lower level shall be adopted.</p> <p>Comment – <i>the proposal will comply with recession plane standards on all boundaries.</i></p>	Complies

14.6.2.3 Road boundary building setback	<p>a. For sites fronting Bealey Avenue, buildings shall be setback a minimum of 6 metres from the road boundary of Bealey Avenue;</p> <p>b. In the locations indicated as Central City Building Setbacks, on the Central City Active Frontages and Verandas and Building Setback Planning Map, buildings shall be setback a minimum of 4.5 metres from road boundaries;</p> <p>c. In all other instances, buildings shall be setback a minimum of 2 metres from road boundaries, except that:</p> <p>i. Where a garage has a vehicle door facing a road, the garage door shall be setback a minimum of 4.5 metres unless the garage door projects outward, in which case the garage door shall be setback a minimum of 5.5 metres;</p> <p>ii. Where a garage has the vehicle door facing a shared accessway, the garage door shall be setback a minimum of 7 metres measured from the garage door to the furthest formed edge of the adjacent shared access unless the garage door projects outwards, in which case the garage door shall be setback a minimum of 8 metres;</p> <p>iii. For street fronting residential units, garages, carports, and other accessory buildings (excluding basement parking areas and swimming pools) shall be located at least 1.2 metres further from the road boundary than the front facade of any ground level habitable space of that residential unit.</p> <p>Comment – all buildings will be setback a minimum of 2m from the road boundary. No garages will face or adjoin the road.</p>	Complies
14.6.2.4 Minimum building setbacks from internal boundaries	<p>a. Buildings that immediately adjoin an access lot, access strip, or access to a rear site shall be setback a minimum of 1 metre from that part of an internal boundary of a site.</p> <p>b. Buildings shall be setback a minimum of 1.8 metres from other internal boundaries of a site, except that:</p> <p>i. no setback is required from an access lot or access strip on the same site, provided that any windows on the ground floor facing and within 1 metre of the access lot or access strip are non-opening;</p> <p>ii. no setback for accessory buildings is required, provided the total length of walls or parts of accessory buildings facing and located within the setback is less than 10.1 metres and/or where the accessory building faces the ground floor window of a habitable space on the adjoining site it shall be setback a minimum of 1.8 metres from that neighbouring window for a minimum length of 2 metres either side of the window;</p> <p>iii. no setback is required along that part of an internal boundary where buildings on adjoining sites have a common wall along the internal boundary;</p> <p>iv. no setback is required for basements, provided that any part of a basement located within 1.8 metres of an internal boundary is wholly below ground level.</p> <p>c. Parts of a balcony or any window of a living area at first floor level or above shall not be located within 4 metres of an internal boundary of a site, except that this shall not apply to a window at an angle of 90o or greater to the boundary, or a window or balcony which begins within 1.2 metres of ground level (such as above a garage which is partly below ground level).</p> <p>d. For the purposes of this rule this excludes guttering up to 200mm in width from the wall of a building.</p> <p>Comment – all buildings will be setback a minimum of 1.8m from internal boundaries. No first floor living area windows or balconies will be located within 4m of an internal boundary.</p>	Complies
14.6.2.5 Fencing and screening	<p>a. Parking areas shall be screened on internal boundaries by landscaping, wall(s), fences(s), or a combination of these to a minimum height of 1.5m from any adjoining site. Where this screening is by way of landscaping it shall be for a minimum depth of 1.5m and the minimum height shall be the minimum height at the time of planting:</p> <p>b. Other than for screening of the required area of service space or outdoor living space, fences and other screening structures shall not exceed 1m in height where they are located either:</p>	Does not comply

	<p>i. within 2m of the road boundary; or</p> <p>ii. on the boundary with any land zoned Open Space Community Parks Zone, Open Space Water and Margins Zone and Avon River Precinct/Te Papa Otakaro Zone, except that the maximum height shall be 2m if the whole fence or screening structure is at least 50% transparent.</p> <p>c. For the purposes of this rule, a fence or other screening structure is not the exterior wall of a building or accessory building.</p> <p>Comment – both parking areas will be screened on internal boundaries by fences 2m in height. The 2m high fence will extend to the road boundary.</p>	
14.6.2.6 Tree and garden planting	<p>a. A minimum of 20% of the site shall be provided for landscaping (which may include private or communal open space in residential developments, where:</p> <p>i. at least 50% of the landscaping shall be trees and shrubs, and</p> <p>ii. a minimum of one native tree for every 250m² of gross site area (prior to subdivision), or part thereof, is included within the landscaping;</p> <p>b. all trees shall be not less than 1.5m high at the time of planting;</p> <p>c. all trees and landscaping required by the rule shall be maintained and if dead, diseased or damaged, shall be replaced.</p> <p>Comment –21.6% of the site will be provided for soft landscaping (at both ground and upper floor levels but excluding the market garden). An additional 30.4% of the site will comprise courtyards, paths etc. 43 native trees will be planted (1 tree per 103m²). 41% of landscaping will be in trees and shrubs. All trees will be not less than 1.5m at the time of planting.</p>	Does not comply
14.6.2.7 Minimum residential unit size	<p>a. The minimum net floor area (including toilets and bathrooms) for any residential unit (excluding parking areas, garages, or balconies allocated to each unit) shall be:</p> <p>i. Studio 35m²</p> <p>ii. 1 Bedroom 45m²</p> <p>iii. 2 Bedroom 70m²</p> <p>iv. 3 or more Bedrooms 90m².</p> <p>Comment – The residential units (1 x 1 bed manager's unit, 4 x 3 bed, 1 x 4 bed) all exceed the minimum net floor area requirements.</p>	Complies
14.6.2.8 Ground floor habitable space	<p>a. Any residential unit fronting a road or public open space, unless built over an access way or another residential unit, shall have a habitable space located at ground level.</p> <p>b. At least 30% of all residential units within a development shall have a habitable space located at ground level.</p> <p>c. At least one habitable space located at the ground level of a residential unit shall have a minimum floor area of 12m² and a minimum internal dimension of 3 metres.</p> <p>Comment – all residential units have a habitable space at ground floor level. All ground floor habitable spaces will exceed 12m² and a minimum dimension of 3m.</p>	Complies
14.6.2.9 Outdoor living space	<p>a. Each residential unit shall provide onsite an outdoor living space of at least 24m².</p> <p>b. The required outdoor living space for each residential unit can be provided through a mix of private and communal areas, at the ground level or in balconies, provided that:</p> <p>i. each residential unit shall have private outdoor living space of at least 8m² in total, not occupied by parking areas or access;</p> <p>ii. each private outdoor living space dimension shall be a minimum of 4m when provided at ground level and a minimum of 1.5 metres when provided by a balcony;</p> <p>iii. at least one private outdoor living space is to be directly accessible from a living area of that residential unit;</p>	Does not comply

	<p>iv. each outdoor living space provided as a communal space shall be accessible for use by all on site residents and each dimension shall be a minimum of 4 metres;</p> <p>v. 50% of the outdoor living space required across the entire site shall be provided at ground level;</p> <p>Comment – <i>The proposed residential units (x6) have a communal outdoor living area at ground floor level of approximately 220m², exceeding the 144m² total outdoor living space required. At least 144m² will have a minimum dimension well in excess of 4m. Each residential unit will have a minimum of 8m² private outdoor living space but with a minimum dimension of less than 4m (2.7m) and the private outdoor living space will not be directly accessible from the living area for the 3 and 4 bed units.</i></p>	
14.6.2.10 Service space	<p>a. Each residential unit shall be provided with at least 3m² of indoor or outdoor service space at ground floor level for the dedicated storage of waste and recycling bins.</p> <p>b. The required service space for each residential unit shall be provided either individually, or within a dedicated shared communal space, but shall not be located between the road boundary and any habitable space.</p> <p>c. Service space for the storage of waste and recycling bins shall be fully screened from any site, road and outdoor living space which adjoins the service space.</p> <p>Comment - <i>Each of the 3 and 4 bedroom residential units will have 2.5m² outdoor service space. The 1 bedroom on site manager's unit will have 3m² outdoor service space. All service space areas will be screened and will be private.</i></p>	Does not comply
14.6.2.11 Minimum site density from development and redevelopment of residential units	<p>a. The minimum residential site density to be achieved when a site is developed or redeveloped with a residential unit or units shall be not less than one residential unit for every complete 200m² of site area (e.g. a site area of 399m² requires 1 residential unit, a site area of 400m² requires 2 residential units).</p> <p>Comment – <i>A density of 22 residential units is required, 6 residential units are proposed (noting an additional 24 bedrooms for supervised accommodation are proposed that do not fall within the definition of a residential unit).</i></p>	Does not comply
14.6.2.12 Water supply for firefighting	<p>a. Provision for sufficient water supply and access to water supplies for firefighting shall be made available to all buildings (excluding accessory buildings that are not habitable buildings) via Council's urban reticulated system (where available) in accordance with the New Zealand Fire Service Firefighting Water Supplies Code of Practice (SNZ PAS 4509:2008).</p> <p>Comment – <i>it is assumed that the water supply complies.</i></p>	Complies

Appendix 6

Urban Design Panel Feedback

Christchurch Urban Design Panel

Proposal Recommendations

3 SEPTEMBER 2019

By Email: Jeremy@nobogroup.co.nz

Dear: Jeremy Phillips

Urban Design Panel – 109 Salisbury Street – Christchurch Youth Hub

The above Panel considered your application on 3 September 2019. Please find below the confirmed comments from that meeting.

In response to the material circulated and the review meeting, the Panel thanks the applicant for their attendance and the narrative that has been developed around the project. The panel supports a use such as this in this location due to its centrality, access to public transport and connectivity to local services and other key amenities within the city. We see the benefit of bringing together such a wide range of services in one location for the youth of Christchurch.

We commend the applicant for demonstrating their intention to be a good neighbour and looking to integrate the proposal into its residential context in a sympathetic manner.

A. KEY DESIGN AND CONSENT RECOMMENDATIONS:

"Recommendations on matters to be addressed for Panel support of the application"

The Panel:

1. Encourages the applicant to consolidate the proposed café, gallery and on-site produce shop in order to maximise the opportunity for activity on Salisbury Street for the benefit of the wider neighbourhood.

This could also enhance the built form on this façade and give it more strength.

2. Appreciates that the buildings on the northern boundary may comply with the planning controls, however the panel recommends that the setback to the boundary be increased and mitigated through a landscape component to reduce possible visual impact on its northern neighbours.
3. Supports basketball within this facility, however see a conflict between the basketball activities at rooftop level with the surrounding residential context, particularly with regards to noise and visual impact being out of context.
4. Supports the proposal for the productive planting, for its training opportunities and positive contribution to the environment. We encourage the continued development of the proposal, including fruit trees for variation in landscape scale and in lieu of additional car parking.
5. Recommends seeking traffic engineering advice to support the reduction in car parking numbers and increasing the provision of secured storage for micro-transport such as (e) bikes and scooters

Christchurch Urban Design Panel

Proposal Recommendations

6. Encourages reconsideration of the internal organisation of the access spine corridor and the potential to open up to the courtyards and internal spaces for improved legibility / wayfinding and variation of the journey through the facility.
7. Encourages the development of sustainable design strategies including water management, planting, energy systems, construction materials and alternative transport.

B. SECONDARY RECOMMENDATIONS:

"Further improvements and value added recommendations"

The Panel:

8. Questions the feasibility of the extensive green roofs and suggests considering a more modest approach and incorporating plants in a variety of ways.
9. Notes colour of the buildings / roofs can be managed to ensure visible outer coherence with the surrounding context while offering vibrancy in internal or inward facing surfaces.
10. Commends the holistic nature of the development, the inclusion of Māori kaupapa, engagement with mana whenua organisation Matapopore and its resonance with Te Whare Tapa whā (Mason Durie), and we encourage the creative exploration of water as a spiritual, healing and cleansing element across the site.
11. Encourages increased attention to the presence of the facility on the Salisbury Street frontage, highlighting the welcoming nature of the entry and the embedded notion of manaaki that sits at the heart of this facility.

Christchurch Urban Design Panel

Proposal Recommendations

PLEASE NOTE:

The Urban Design Panel is an advisory body only. The Panel has no statutory decision making powers. The Panel's recommendations are to assist you in the refinement of your development proposal and the reporting Council officer will take its advice into account when processing any resource consent applications. The decision on any application rests with the Council.

The Christchurch City Council understands that you may wish to refer to the Urban Design Panel recommendations in the promotion of your development proposal. Please note the comments are not intended for publication.

To further discuss the Panel's recommendations please contact Nicola Williams (Council Urban Design Panel facilitator) at Nic.Williams@ccc.govt.nz.

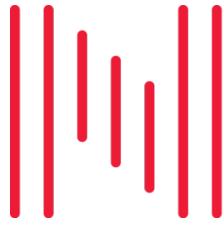
Please feel free to contact me in regards to any administrative matters (as the Council Urban Design Panel administrator) at Bernice.JansevanRensburg@ccc.govt.nz

Yours sincerely

Bernice Janse van Rensburg
Hearings & Council Support Officer
Community Support, Governance & Partnerships Unit
Customer & Community Group

Appendix 7

Transport Assessment



NOVO group
Planning. Traffic. Development.

Integrated Transport Assessment
Prepared for

**TE HURIHANGA Ō
RANGATAHI – THE
YOUTH HUB**

**109 Salisbury Street,
Christchurch**

February 2020



Integrated Transport Assessment
Prepared for

Te Hurihanga ō Rangatahi – The Youth Hub

109 Salisbury Street,
Christchurch

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- Appendix 1 Application Plans
- Appendix 2 Transport Compliance Assessment
- Appendix 3 Parking Demand by Time
- Appendix 4 Travel Management Plan Framework
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Introduction

1. Te Hurihanga o Rangatahi – The Youth Hub has commissioned Novo Group to prepare an Integrated Transport Assessment (ITA) for the development of a youth focussed community facility at 109 Salisbury Street in central Christchurch.
2. This report provides an assessment of the transport aspects of the proposed development. It also describes the transport environment in the vicinity of the site, the transport related components of the proposal and identifies compliance issues with the transport provisions in the District Plan. It has been prepared broadly in accordance with the Integrated Transportation Assessment Guidelines specified in New Zealand Transport Agency Research report 422, November 2010.
3. It is proposed to develop the site at 109 Salisbury Street to provide for a range of activities including residential, a market garden and community facility incorporating entertainment, health care and office, plus ancillary food and beverage. The activity will be supported by ten car parking spaces (including two mobility spaces) in two parking areas, with access to Salisbury Street and Gracefield Avenue.
4. The proposed activities are predicted to generate in the range of 34 to 83 vehicles per hour during the peak periods. The peak parking demand is predicted to be 49 to 71 vehicles.
5. The site location is illustrated in **Figure 1** and a copy of the proposed site layout is contained in **Appendix 1**.



Figure 1: Site Location



Transport Environment

Road Network

Salisbury Street

6. The details of Salisbury Street are set out in **Table 1**. An image of Salisbury Street is included as **Figure 1**.

Table 1: Salisbury Street Details

Key Feature or Characteristic	Comment
Road Classification	Central City Local Distributor
Cross-Section Description	Two eastbound through lanes plus on-street car parking on both sides of the road.
Traffic Volumes	No traffic counts have been undertaken since 2014. Mobile Road estimates from 2017 suggest a daily volume of 3,556 vehicles per day. This leads to a peak hour of approximately 356 vehicles per hour.
Speed	50km/hr.
Cycling Infrastructure	Wide parking lane that informally accommodates cycling.
Pedestrian Infrastructure	Footpaths provided on both sides.
Public Transport	Routes 29, 95 and Blue Line
Parking	All day car parking permitted. There is capacity to accommodate approximately 45 vehicles parking on street between Durham Street and Colombo Street. A weekday afternoon survey (at approximately 14:30) identified that 38 of these spaces were occupied. A weekday evening survey (at 18:00) identified that 29 of these were occupied.
Road Safety	No crashes have been reported in the vicinity of the site in the most recent five-year period available.
Additional Notes	It is understood that Salisbury Street may become a two lane two-way road. This would provide on-street cycle lanes and on-street car parking on both sides. We are not aware of a date for further design or construction for this project.



Figure 2: View of Salisbury Street



Gracefield Avenue

7. The details of Salisbury Street are set out in **Table 2**. An image of Salisbury Street is included as **Figure 3**.

Table 2: Gracefield Avenue

Key Feature or Characteristic	
Road Classification	Local Road
Cross-Section Description	Varies, but includes a two-way carriageway of approximately 7.0m that accommodates on-street car parking on the eastern side only on the north-south section along the site boundary.
Traffic Volumes	Mobile Road estimates from 2017 suggest a daily volume of 154 to 300 vehicles per day. This leads to a peak hour of approximately 15 to 30 vehicles per hour.
Speed	50km/hr.
Pedestrian Infrastructure	Footpath only on eastern side of the road for the southern 87m of the north-south link along the site boundary. Footpaths on both sides north of this.
Parking	All day car parking permitted. There is capacity to accommodate approximately 49 cars parked on-street. A weekday afternoon survey (at approximately 14:30) identified that 35 of these spaces were occupied. A weekday evening survey (at 18:00) identified that 31 of these were occupied.
Road Safety	No crashes have been reported on this road or at its intersections in the most recent five-year period available.
Additional Notes	On-street parking leads to single lane traffic flow on the north-south section along the site frontage.



Figure 3: View of Gracefield Avenue



Alternative Transport Modes

Passenger Transport

8. A bus stop is located on Salisbury Street 50m east of the application site. **Table 3** summarises the services using this stop.

Table 3: Passenger Transport Services

Route	Frequency
Blue Line: Belfast to Rangiora via Bus Interchange	Every 15-minutes
Route 29: Christchurch International Airport to Bus Interchange	Every 30-minutes
Route 95: Pegasus & Waikuku to Bus Interchange	Hourly

Cycling

9. The key cycle infrastructure in the vicinity of the application site is illustrated in **Figure 4**. This indicates that the site is within reasonable proximity to cycle lanes. It is also likely that the Major Cycleway on Colombo Street (that terminates at Bealey Avenue) will provide a good level of service for the site and Colombo Street within the Central City is identified on the Council’s Major Cycleways scheduling map as being part of the Major Cycleways network.

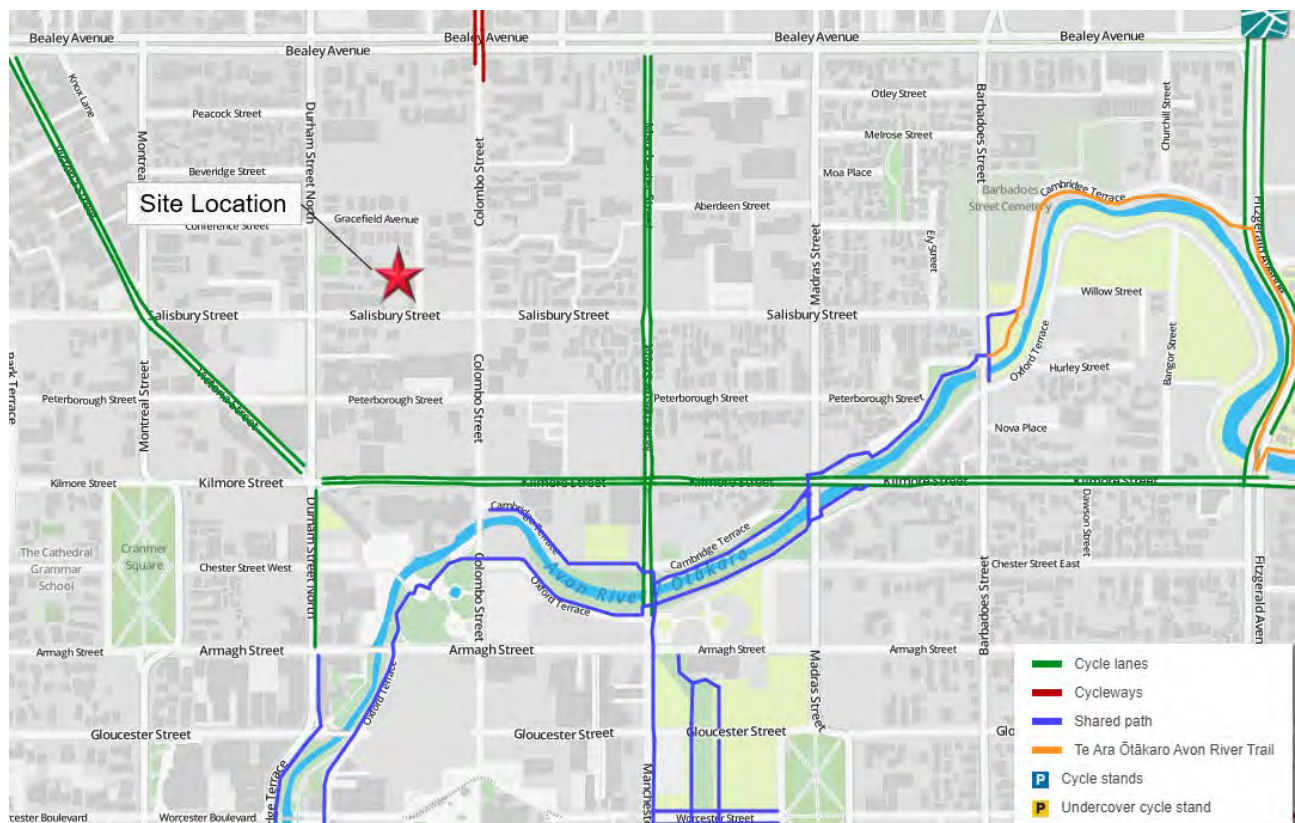


Figure 4: Cycle Infrastructure



Surrounding Off-Street Car Parks

10. The closest publicly available off-street car park to the application site is at the Christchurch Casino. The car parking is located in the corner of Salisbury Street and Durham Street North, which is approximately 140m walk from the application site. There are approximately 350 car parks provided and the typical occupancy of these spaces is no more than 60% (i.e. there are typically at least 140 available spaces).
11. This car parking is currently free for the first two hours and then \$3 per hour.

The Proposal

12. The proposed activity is understood to comprise the following:
 - i. 6 residential units;
 - ii. 24 sheltered housing units;
 - iii. 119m² PFA (68m² GFA) of food and beverage (café);
 - iv. 737m² PFA (741m² GFA) of entertainment space;
 - v. 231m² of health care facility (including counselling);
 - vi. 419m² GFA of office (including meeting room space);
 - vii. 133m² basketball court; and
 - viii. 779m² GFA (191m² PFA) of market garden area.
13. It is understood that the proposed activity has a particular focus on youths and youth counselling. As such, it is understood that the majority of visitors will not be driving. The site layout includes ten car parking spaces and vehicle access to Salisbury Street and Gracefield Avenue.
14. The following sets out the transport details of the proposed activity.

Car Parking Demands

15. Several approaches have been undertaken to estimating the car parking demands for the proposed activities, as set out below.

District Plan Requirements

16. Although the application site is within the Central City and the District Plan does not require car parks, a comparison has been made against the District Plan parking requirements including the Permitted Reduction Factors. The full calculation is set out in **Appendix 2**.
17. The calculated car parking requirement for the activity on the basis of the above is five residential car parks, 54 visitor car parks and 12 staff car parks (71 car parks in total).



Demand Based Assessment

18. A second methodology has been applied, which uses survey data to determine the anticipated car parking demand. This data is summarised in **Table 4**.

Table 4: Predicted Car Parking Demands

Activity	Car Parking Rate	Source	Car Parking Demand
Apartments (30 Units ¹)	1 space per 3 units	CCC Social Housing Report	10
Café (68m ² GFA)	12 per 100m ² GFA	Survey of The Cup	8
Event Space (737m ² GFA)	3.4 to 5.4 per 100m ² GFA	ITE data for <i>Recreational Community Centre</i>	25 to 40
Health care (231m ² GFA)	4.2 to 6.0 spaces per 100m ² GFA	NZ Trips and Parking Database for Medical Centres	10 to 14
Office (419m ² GFA)	1 per 40m ² GFA	RTA Guide to Traffic Generating Developments	10
Basketball (1 Court)	Assume 8 per court	Assumed to be ancillary to other on-site activities.	-
Market garden	Assumed 24 vehicles	Based on client advice of 12 staff and 25 visitors, then applying a 64.1% car driver mode share ²	24
Total	-	-	87 to 106

19. The above indicates that the car parking demands would be in the order of 87 to 106 vehicles at peak times. However, this is based on suburban parking surveys and is not considered to be reflective of a Central City location. As such, the 54% Permitted Reduction Factor (based on the District Plan methodology) has been applied as a proxy to acknowledge the site location and that the above is based on sites with unconstrained car parking. This leads to a suggested car parking demand for 40 to 49 car parking spaces.
20. This data has also been split in accordance with the anticipated timing of demands, based on the following assumptions:
- i. The residential parking demand could occur all day, seven days per week;
 - ii. The Youthline call centre staff would be at the site between 08:00 and 00:00, seven days per week;
 - iii. The Youthline office staff will be present between 07:00 and 18:00, seven days per week;
 - iv. The office meeting spaces will be available between 08:00 and 17:00, plus associated evening use for board meetings and training;

¹ Assumes that all the residential will be similar to social housing overall, as we are not aware of specific data for the housing types / used proposed.

² 64.1% car driver mode share is the average for Christchurch City as a whole according to NZ Statistics data.



- v. The café parking demand would likely be between 09:00 and 17:00, seven days per week;
- vi. The market garden parking demands are understood to occur between 08:00 and 16:00, weekdays;
- vii. The recreational activities will be open 09:00 to 21:30 seven days per week;
- viii. The event space would be open for typically up to 60 people between 09:00 to 21:30 seven days per week; and
- ix. Other agencies at the site will operate between 09:00 and 17:00 on weekdays.

21. **Appendix 3** sets out a distribution of the car parking demands over time and **Figure 5** provides a summary of this³. This indicates a peak parking demand of 49 vehicles occurring from 09:00 to 16:00.

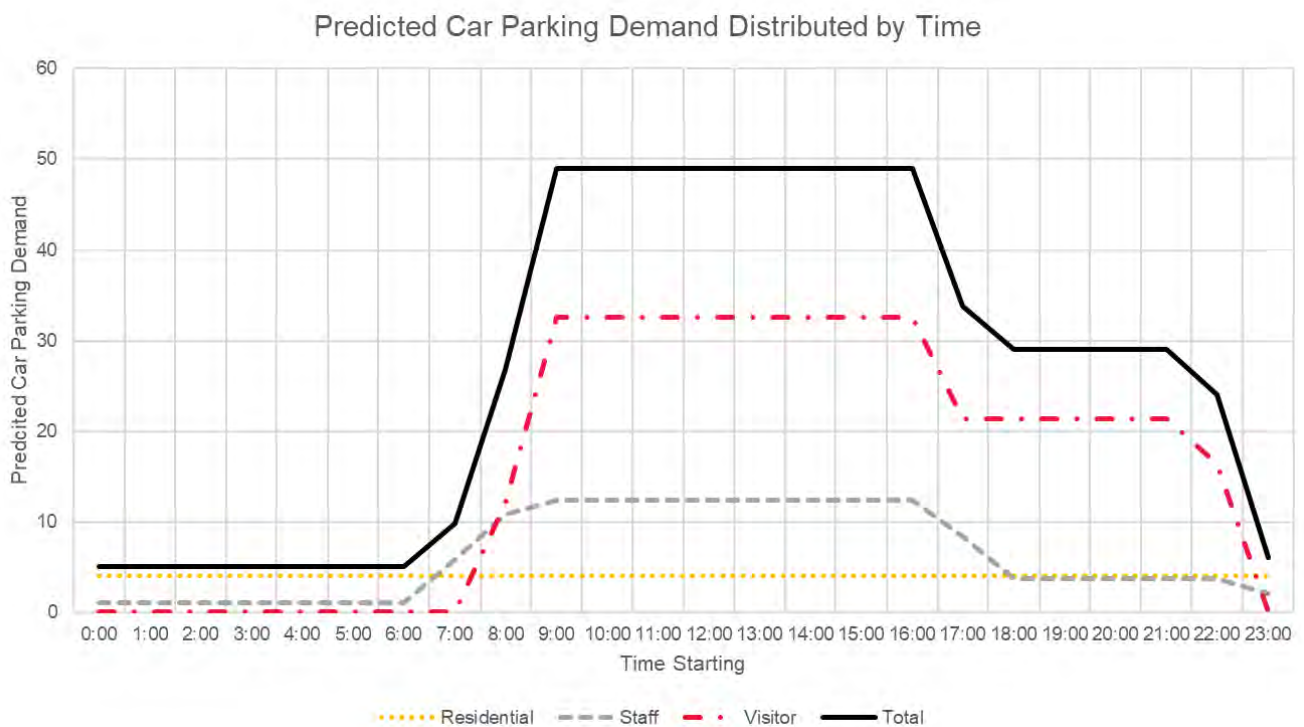


Figure 5: Distributed Car Parking Demands – Demand Based

Overall Car Parking Demands

22. The above information suggests that the car parking demand is anticipated to be in the range of 49 to 71 vehicles.

³ The apportioning of demands between resident, staff and visitor is based on District Plan requirements as this is not separated out in the survey data.



Traffic Generation

23. The traffic generation of the proposed activity has been based on the data set out in **Table 5**. The summary at the bottom of the table assumes that the traffic generation of the activity is reduced by 54%, as per the car parking assessment.

Table 5: Traffic Generation

Activity	Traffic Generation Rate	Source	Vehicle Movements per Hour
Apartments (30 Units)	0.3 vehicle movements per unit per hour.	NZ Trips and Parking Database.	9 vehicle movements
Café (68m ² GFA)	5 vehicle movements per 100m ² GFA	RTA Data for Restaurants	3 vehicle movements
Event Space (737m ² GFA)	1.56 vehicle movements per 100m ² GFA	ITE data for <i>Recreational Community Centre</i>	11 vehicle movements
Health care (231m ² GFA)	8.3 to 12.2 vehicle movements per 100m ² GFA	NZ Trips and Parking Database for Medical Centres	19 to 28 vehicle movements
Office (419m ² GFA)	2 vehicle movements per 100m ² GFA	RTA Data for Offices	8 vehicle movements
Basketball (1 Court)	Assumed to be ancillary to other activities.		-
Market Garden	24 vehicles per hour	Based on applicants parking estimate	24
Total	-	-	74 to 83 vehicle movements per hour.
Reduced Total (54% reduction to account for central city location)			34 to 38 vehicle movements per hour

24. The traffic generation of 34 - 38 vehicle movements per hour is similar to the predicted parking demand for 49 vehicles, set out at paragraph 22. It is acknowledged this is low when compared to the overall parking demand range of 49 – 71 vehicles, so the unfactored data of 74 – 83 vehicles per hour represents the higher end of the potential traffic generation.
25. The distribution of vehicles by trip type (i.e. resident / staff / visitor) is not provided in the survey data, so the District Plan car parking ratios has again been used as a guide to split out the trip types. These are summarised in **Table 6**.



Table 6: Peak Hour Vehicle Trips by Purpose

Activity	Resident	Staff	Visitor	Total
Apartments	3 – 8	1	0	4 – 9
Café	0	0	1 – 3	1 – 3
Event Space	0	0 – 1	5 – 10	5 – 11
Health care	0	2 – 6	7 – 22	9 – 28
Office	0	4 - 8	0	4 – 8
Basketball	0	0	0	0
Market garden	0	4 - 8	7 - 16	11 – 24
Total	3 - 8	11 - 24	20 - 51	34 – 83

Site Layout

Access Arrangements – Salisbury Street

26. The Salisbury Street access is located on the western site boundary, approximately 42m west of the intersection with Gracefield Avenue. This access is proposed to serve four car parking spaces, plus the loading bay / lift for the community garden. The access is proposed to be 6.6m wide and includes a 5.0m by 2.0m visibility splay on the eastern side. A queue space of 5.0m is provided.
27. The car parking in this area is proposed to be for visitors only.

Access Arrangements – Gracefield Avenue

28. The Gracefield Avenue access is toward the northern boundary of the site, approximately 69m north of Salisbury Street. This access leads to six car parking spaces. The access is proposed to be 3.3m wide with a gate approximately 4.4m back from the road boundary. Additional width is provided for pedestrians on both sides of this access.
29. The car parking in this area will be for residents and staff.

Car Parking & Loading

30. The car parking accessed via Salisbury Street will be for visitors. The dimensions of these car parking spaces are typically 2.5m wide, 5.0m long and the aisle is 6.6m. The mobility space will be 3.6m wide, 5.0m long and also has an aisle of 6.6m.
31. The car parking accessed via Gracefield Avenue will be for staff and residents. The dimensions of these car parks are typically 2.4m wide, 5.0m long and the aisle is 6.6m. The mobility space is 3.6m wide, 5.0m long and the aisle is again 6.6m and the visitor space is 2.5m wide, 5.0m long with a 6.6m aisle. Additional manoeuvring is anticipated to be required because the access is 3.3m wide and centrally located (meaning that the full 6.6m aisle is not available for entering the parking spaces).
32. A lift is proposed in the Salisbury Street car park that will enable materials to be taken to / from the rooftop community garden. It is proposed that the adjacent car park will be managed to be available to accommodate loading for the site on an as required basis.



Cycle Parking

33. A total of 95 cycle parking spaces are proposed at the site. This includes 17 visitor cycle parking spaces near the Salisbury Street entrance, 36 visitor cycle parks in the building accessed via the Salisbury Street access and 42 resident / staff cycle parks accessed via Gracefield Avenue. The following figures illustrate the layout of these cycle parking spaces.

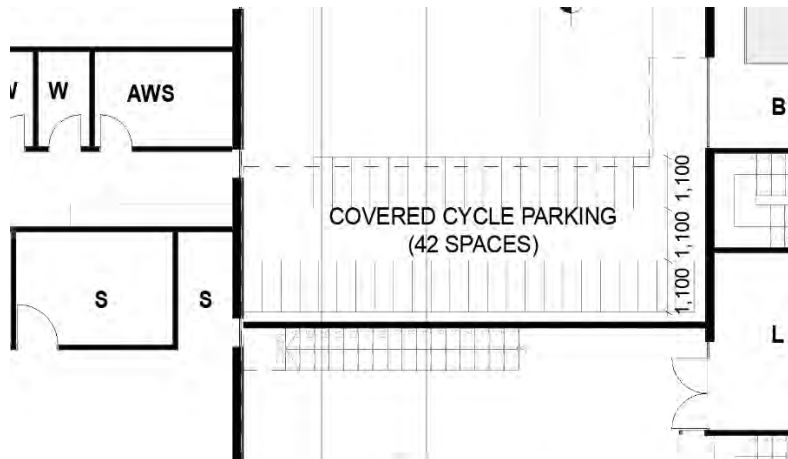


Figure 6: Gracefield Avenue Staff / Resident Covered Cycle Parking

34. **Figure 6** indicates that a wall-mounted vertical cycle parking system is proposed that includes a 1.1m aisle for the resident / staff cycle parking.

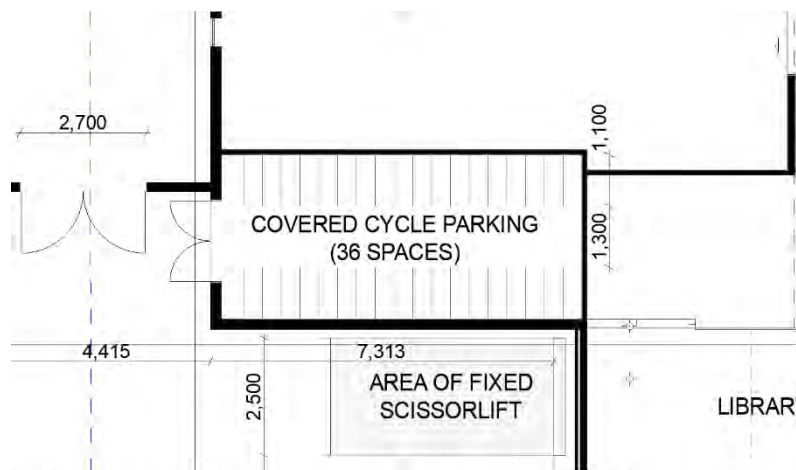


Figure 7: Salisbury Street Covered Cycle Parks

35. **Figure 7** illustrates that a wall-mounted vertical cycle parking system is proposed that includes a 1.3m aisle for the covered visitor cycle parking

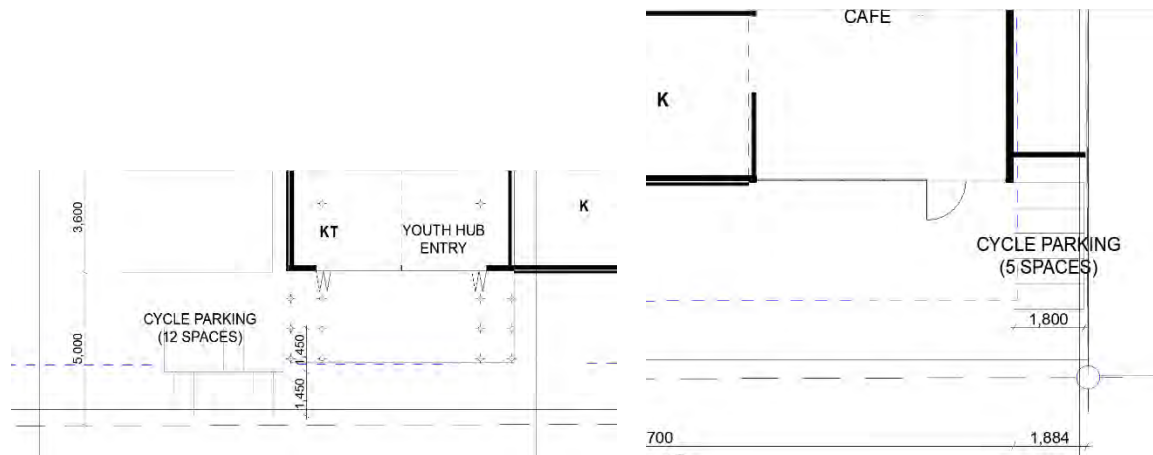


Figure 8: Salisbury Street Visitor Cycle Parking

36. The uncovered visitor cycle parking on Salisbury Street complies with the layout requirements of the District Plan (noting that the twelve spaces are proposed to be staggered).

District Plan Compliance Assessment

37. The site is zoned *Central City Residential* in the District Plan and the proposed activity is understood to be *Non-Complying* in the zone. An assessment of compliance against the transport rules of the District Plan has been undertaken and is contained in **Appendix 2. Table 7** summarises the non-compliances identified.

Table 7: District Plan Transport Non-Compliance

Rule	Nature of Non-Compliance
7.4.3.2 Minimum number of cycle parking facilities required	The layout of the cycle parks does not comply with the District Plan requirements.
7.4.3.4 Manoeuvring for parking & loading areas	Additional manoeuvring is required to enter and exit parking spaces accessed via Gracefield Avenue.
7.4.3.7 Access design	The Gracefield Avenue access does not provide the required pedestrian visibility splay and is narrower than the required width (3.3m compared to 4.0m required).
7.4.3.10 High trip generators	The proposed activity has been estimated as generating 34 to 83 vehicle movements per hour, so assessment is required against the Basic ITA matters.
7.4.3.11 Vehicle access to sites fronting more than one street – Within the Central City	Access should only be taken from Salisbury Street, whereas access is also proposed to Gracefield Avenue.

Assessment of Effects

38. The proposed activity is understood to be *Non-Complying* in the zone, so an assessment of all transport matters is required. The key matters for assessment are considered to be:
- Parking and Loading:** The adequacy of parking and loading at the site, as well as the layout of the parking and loading arrangements;



- ii. **Access Arrangements:** The safety and efficiency of the proposed site access arrangements; and
- iii. **Wider Network Effects:** The effects of the proposed activity on the wider transport network.

39. The above, plus the specific non-compliances identified in **Table 7**, are assessed in turn below.

Parking and Loading

Car Parking Supply and Demand

- 40. The District Plan does not require car parking within the Central City and an effective maximum parking allowance is provided within most Central City zones⁴. That said, the proposed activity is *Non-Complying* and this enables consideration of the car parking supply and demand.
- 41. The car parking supply proposed at the site is ten spaces. These comprise:
 - i. Four visitor spaces (accessed via Salisbury Street); and
 - ii. Six resident / staff spaces (accessed via Gracefield Avenue).

Resident Car Parking

- 42. The very limited number of resident spaces proposed is considered to be sufficient to practically meet the demands of the people living at the site. The accommodation is primarily intended for displaced teens who the client advises are unlikely to have vehicles. The residential car parking proposed is effectively for the caretaker and overnight managers, who would also work at the site, with no car parking being available for the other residents.

Staff Car Parking

- 43. Although six staff / resident car parks are proposed, it has been estimated that four of these would be used by the staff residing at the residential component. Although there may be some cross-over of car park use, this provides potentially only two staff car parks compared to a demand for approximately twelve spaces. This in turn leads to a shortfall of ten staff car parking spaces and these people will need to find alternate car parking locations. The on-street car parking review (included in **Table 1** and **Table 2**) indicated the on-street car parking on the surrounding roads is largely near capacity throughout the working day. In addition, there is capacity for approximately 73 cars to park on Durham Street between Bealey Avenue and Salisbury Street. Spot checks indicated these are fully occupied during the weekday, although there was capacity for approximately 20 vehicles to park here after 18:00. As such, staff are unlikely to find on-street car parking in the vicinity of the application site, or their on-street parking would displace other all day car parking.
- 44. This is essentially the same as for the majority of people employed within the Central City. The logical location for the car parking to occur is the public off-street car parks, such as the Casino car park. There is ample space to accommodate that car parking and this is considered to be acceptable given the Central City location of the application site. The lack of parking will also encourage staff to travel by modes other than single car occupant (i.e. as car passengers, walking, cycling and public transport).

⁴ All zones other than the Central City Residential zone have an upper limit on the area available for car parking as 50% of the GLFA of the activity proposed.



Visitor Car Parking

45. A demand for approximately 33 visitor car parks has been estimated throughout the majority of the day, compared to a provision of four visitor spaces. As with the staff car parking, these visitors are unlikely to find on-street car parking in the vicinity of the site during the day. The availability of car parking does increase after 18:00, although this is potentially a short walk from the site, as it is on Durham Street as well as Salisbury Street.
46. As with the staff car parking, visitors will be able to make use of the car parking at the Casino, which is currently free for the first two hours and should therefore be an attractive option. Some drop-off activity is anticipated to occur at the site. The site provides four visitor spaces, so management of these spaces will be required when events are taking place to ensure pick-up / drop-off parking occurs in an organised manner so that it does not affect the through flow on Salisbury Street.

Travel Management Plan

47. It is proposed to have a Travel Management Plan with the aim of reducing the number of car trips to the application site and to manage car parking associated with those people that do travel by car. A framework/template of the measures that may be included within the Travel Management Plan is set out in **Appendix 4**. The key actions are aimed at:
 - i. Reducing car travel to the site through the provision of measures to encourage the use of alternative transport modes;
 - ii. Promoting adjacent off-street car parks and discouraging on-street parking from Gracefield Avenue in particular; and
 - iii. Management of the car parks at the application site.

Alternate Development Scenario

48. The District Plan requires a minimum of one residential unit per 200m² of site area if it were to be developed for solely residential activities. Applying this density to the approximately 4,450m² application site indicates in the order of 22 residential units could be developed without providing car parking. This in turn indicates there is a baseline for at least 22 vehicles parking on-street in the surrounding area.
49. The above example is a minimum requirement for the site, which would more realistically be more intensively developed. As a comparison, residential townhouse development has been approved at 257 Armagh Street without any car parking provided. That site had an average ratio of one unit per 90m² site area. Applying this density to the approximately 4,450m² application site indicates in the order of 49 residential units could feasibly be developed without providing car parking.
50. Another example is the apartment development at the Hagley Avenue / Moorhouse Avenue intersection. This has an average density of one unit per 50m² and also does not provide car parking. This would equate to approximately 89 units if this density were achieved on the application site.
51. The above indicates there is a baseline for at least 22 vehicles to park on-street, assuming there is a demand for one car park per residential unit. This could more realistically be in the range of 49 to 89 vehicles parking on-street with a more intensive development scenario. This range of car parking demands is broadly consistent with that identified for the proposed activities.



Car Parking Summary

52. It is acknowledged that the site will not provide sufficient car parking to accommodate the likely demand. This is consistent with the majority of Central City activities in Christchurch and will encourage travel by alternate transport modes. A Travel Management Plan is proposed to further encourage and facilitate the use of modes other than private car travel, as far as is practicable.
53. Although the use of on-street car parking is likely, this is unlikely to alter the surrounding transport environment as the existing on-street car parking is already highly occupied. Drivers will be discouraged from seeking parking on Gracefield Avenue and will be encouraged to use surrounding off-street car parks (such as at the Casino); or use alternative modes and means to get to and from the site.
54. The on-site visitor car parks will be managed during events to ensure they provide for pick-up / drop-off trips, so these vehicles do not lead to congestion on Salisbury Street.
55. Given the above, the effects of car parking supply and demand at the application site are considered to be acceptable.

Car Park Layout

56. The car park layout has been identified as complying with the District Plan layout requirements. That said, additional manoeuvring will be required to enter and exit the spaces at the Gracefield Avenue car park. The manoeuvring associated with these car parks is illustrated in **Appendix 5**.
57. The vehicle tracking provided indicates that some parking spaces may be best entered in reverse and additional manoeuvring may also be required to enter / exit some of the spaces. This is because the access width does not match the aisle width of the car park, so vehicles are not able to make full use of the aisle width when entering the parking spaces.
58. Whilst the manoeuvring is not ideal, the spaces requiring additional manoeuvring will be allocated to staff and residents only. These users will become accustomed to the manoeuvring requirements and will tolerate the constraints of this space. On this basis, the Gracefield Avenue car park area is considered to be acceptable.
59. The car park area accessed from Salisbury Street has a layout that complies with the District Plan and the access is such that the full aisle width is available. As such, the manoeuvring associated with this car park is considered to be compliant and acceptable.

Cycle Parking

60. The proposed cycle parking provision at the site will exceed the requirements of the District Plan and is therefore considered to be acceptable. It is also noted that the Travel Management Plan includes a requirement to monitor the use of the cycle parking and increase this if there is the demand and if this is practicable given the constraints of the site.
61. The layout of the cycle parking has been identified as not complying with the District Plan layout requirements. The 17 uncovered visitor cycle stands at the front of the site will comply, so the non-compliance is restricted to the internal cycle stands. These will utilise a wall-mounted system that will require users to lift their cycle onto the back wheel to hang the bike. This type of system is reasonably common in Christchurch where there is limited space, although it is acknowledged it is typically used for staff / resident cycle parking rather than visitors. That said, the demographic visiting Youth Hub are anticipated to primarily be fit and able and therefore will be able to use the proposed cycle stands.



62. Given this, the proposed cycle parking is considered to be acceptable.

Loading

63. Loading at the site is anticipated to be infrequent and a dedicated loading space is not proposed, with management of loading being included in the Travel Management Plan. There will be a lift to the upper level for the community garden and the adjacent space will be made available for this use.
64. Occasional courier deliveries will use the visitor car park. Longer duration deliveries, such as for the set-up of an event, will require management of the visitor car park in advance.
65. The management of the visitor car park to accommodate loading is considered to be a practical way of providing loading at the site when it is required.

Access Arrangements

66. The traffic generation of the site has been estimated as being between 34 and 83 vehicle movements per hour in the peak hour. As such, the assessment matters associated with the Basic ITA requirements has been used as a structure for the following assessment.

***Access and manoeuvring (safety and efficiency):** Whether the provision of access and on-site manoeuvring area associated with the activity, including vehicle loading and servicing deliveries, affects the safety, efficiency, accessibility (including for people whose mobility is restricted) of the site, and the transport network (including considering the road classification of the frontage road).*

Gracefield Avenue Access

67. The Gracefield Avenue access is anticipated to operate safely and efficiently because the volume of traffic using this access is low. The six car parks located here are anticipated to generate approximately three vehicle movements per hour at peak times⁵ - and will only be used by staff / residents. The passing volume on Gracefield Avenue is approximately 15 to 30 vehicle movements per hour, so there will be no concerns regarding the capacity of the access.
68. The visibility for vehicles exiting the access is limited by the proposed wall to the north of the access. Vehicles exiting the site will need to nose out slowly and the emphasis will be on a southbound vehicle on Gracefield Avenue reacting and these drivers will have a good view of vehicles edging out of the site. The existing road alignment encourages slow speeds on this segment of Gracefield Avenue, so this is considered to be an acceptable arrangement.
69. There are no obstructions to the visibility to the south (out of the access), so drivers will be able to identify and react to northbound traffic on Gracefield Avenue.
70. It is also noted there is no footpath on the application site side of the road, so pedestrians are not anticipated to be present at the access.

Salisbury Street Access

71. The Salisbury Street access is anticipated to typically attract approximately eleven vehicle movements per hour at peak times (these are the vehicles associated with the event space). This volume of traffic

⁵ Assumed to be one vehicle movement per hour associated with four residential spaces (i.e. one movement associated with all the spaces based on the traffic generation rate of 0.3 trips per unit) and two staff vehicle movements (i.e. one movement per space).



is anticipated to be readily accommodated by the access as it is reasonably low. In addition, the traffic flow on Salisbury Street is one-way, so vehicles accessing the site will undertake the relatively easy movements of left turn in and left turn out only.

72. A pedestrian visibility splay is provided at the access to enable drivers exiting the site to see pedestrians on Salisbury Street.
73. There may be times when a higher amount of traffic occurs at this access, such as for pick-up and drop-off associated with an event. For the purposes of this assessment, it has been assumed that:
 - i. 200 people would attend an event⁶;
 - ii. There could be one vehicle per three attendees (with the others coming as passengers or by alternate modes);
 - iii. One third of vehicles would be dropping-off passengers and not seek on-street car parking.
74. The above leads to an assumption there could be 44 vehicle movements per hour⁷ associated with the drop-off area at peak times. This suggests that each drop-off space would turn-over every five to six minutes during the hour preceding or following an event. Again, this volume of traffic can be accommodated satisfactorily by the access.

Design and Layout: *Whether the design and layout of the proposed activity maximises opportunities, to the extent practicable, for travel other than by private car, including providing safe and convenient access for travel by such modes.*

75. The proposed activity will provide minimal car parking, which is considered to be the key method of discouraging travel by private car. The proposed amount of cycle parking exceeds the District Plan requirements. A Travel Management Plan is proposed to further facilitate travel by modes other than private car.
76. These measures are considered to be sufficient to the extent practicable for the proposed activity.

Heavy vehicles: *For activities that will generate more than 250 heavy vehicle trips per day, whether there are any effects from these trips on the roading infrastructure.*

77. The proposed activity will generate fewer than 250 heavy vehicle movements per day.

Accessibility of the location: *Whether the proposed activity has demonstrated the accessibility of the site by a range of transport modes and whether the activity's location will minimise or reduce travel to and from the activity by private vehicles and encourage public and active transport use.*

78. The accessibility of the site has been broadly discussed earlier in this report. It is within walking distance of a number of residential properties, particularly given people are anecdotally willing to walk further distances to Central City locations given the lack of available car parking.

⁶ Based on the seating capacity indicated on the plans.

⁷ $200 / 3 = 67$ vehicles. $67 / 3 = 22$ vehicles dropping off. $22 \times 2 = 44$ vehicle movements per hour (i.e. each arrivals arrives and departs).



79. The site is within reasonably close proximity to Major Cycle routes that facilitate safe cycling. The site is also within close proximity to a bus stop that serves several routes, notably the Blue Line that operates on a 15-minute frequency.
80. Overall, the site is considered to be accessible by a range of transport modes.

Vehicle Access to Sites Fronting more than One Street

81. The District Plan requires that access should only be taken from Salisbury Street, whereas an access is also proposed to Gracefield Avenue. The assessment matters for this non-compliance are set out and discussed in turn as follows.

Whether there would be any adverse effects on the safety and amenity of all types of road user passing the site, and/or the function of the frontage road, particularly at times of peak traffic flows on the road and the access;

82. The operation of this access was discussed in paragraphs 67 to 70 and is considered to be acceptable. No adverse effects are anticipated with regards to the function of the frontage road because of the low traffic generation of the activity and the low traffic volumes passing the site. The Gracefield Avenue access only serves six car parking spaces that will only be used for staff and resident purposes – not visitors or the general public.

The extent to which the access disrupts active frontages.

83. It is firstly noted that the site is not within the Central City Active Frontage overlay. The small width of the access given the Gracefield Avenue frontage is also considered to mitigate some concerns regarding this assessment matter. As such, we consider the effects with regards to this matter to be acceptable.

Wider Network Effects

84. The traffic generated by the proposed activity is considered to be spread across the road network surrounding the site because of the limited availability of car parking at the application site. The traffic generation of the proposed activity is estimated as being in the order of 34 to 38 vehicle movements per hour, which can be readily accommodated by Salisbury Street. Even the higher traffic volume estimate of 83 vehicle movements per hour is considered to be able to be accommodated given it will be spread across several routes to differing car parking locations.
85. Overall, the wider network effects of the proposed activity are considered to be acceptable.

Summary and Conclusion

Summary

86. It is proposed to develop the site at 109 Salisbury Street to provide for a range of activities including residential, market garden, a community facility incorporating entertainment, health care and office/meeting space plus ancillary food and beverage. The activity will be supported by ten car parking spaces and includes a Travel Management Plan requirement to minimise the potential effects of car parking.



87. The activity is estimated as generating 34 to 83 vehicle movements per hour in the peak hours. However, these vehicles would not be able to access the site because of the limited car parking supply and would therefore be spread across the wider network as a whole.
88. The activity is predicted to have a car parking demand of between 49 and 71 vehicles at peak times. Again, this demand would not all be confined to the site because of the limited car parking supply and would therefore be spread across other facilities nearby.

Car Parking Demands

89. The site is located within the Central City and the District Plan does not require car parking in this area. That said, the non-complying nature of the activity means that car parking can be considered.
90. The residential component is primarily housing for displaced teens that we have been advised are unlikely to own cars. As such, a minimal residential parking demand is anticipated.
91. Minimal staff car parking will be provided on-site, which means the majority of staff will need to park elsewhere. The surrounding on-street car parking is near capacity, so these drivers will either displace existing car parking or need to find alternate locations. This is considered to be acceptable given this is the way that Central City parking is generally accommodated.
92. Visitors will also likely need to find alternate locations to park if arriving during the working day. The nearby public car parking at the Casino has been identified as a likely location for this to occur. Some vehicles may park on-street if attending an event during the evening, as there is additional capacity for this to occur. The Salisbury Street car park will be managed during events to provide an effective pick-up / drop-off area.
93. Alternate development scenarios have been considered to understand the level of on-street car parking that may occur. The District Plan minimum density requirement could lead to approximately 22 vehicles parking on-street. More intensive scenarios could lead to between 49 and 89 residential vehicles parking on-street and these ranges are similar to the 49 to 71 vehicles anticipated at peak times for the proposed development.
94. Overall, the use of on-street car parking and nearby public off-street car parking facilities is considered to be acceptable because of the Central City location of the site. Given the lack of on-site car parking, there will inevitably be an increased future tendency for staff and visitors to use alternative modes and means to get to and from the site.

Car Park Layout and Manoeuvring

95. The proposed car park layout has been identified as complying with the District Plan requirements. Additional manoeuvring will be required at the Gracefield Avenue car park, although these spaces will be allocated to staff and residents who will come to tolerate this manoeuvring.

Cycle Parking

96. More than sufficient cycle parking is proposed to comply with the District Plan requirements. The cycle parking layout will use wall-mounted stands, although this is considered to be acceptable.



Loading

97. Loading will be accommodated within the Salisbury Street car park. The use of this car park for loading forms part of the Travel Management Plan.

Access Arrangements

98. The operation of the accesses has been reviewed and they are anticipated to operate satisfactorily because of the low traffic generation of the proposed activities and the low volumes passing the site. The Salisbury Street access also benefits from Salisbury Street being a one-way road.

Wider Network Effects

99. The effects of the proposed activity on the wider transport network are anticipated to be acceptable. This is partly because the traffic will be spread across a range of car parking locations, rather than being focussed at the site.

Conclusion

100. Given the above assessment, the transport effects of the proposed activity are considered to be less than minor and acceptable. This is subject to the development of a Travel Management Plan that is consistent with the framework set out in this report.



Appendix 1
Application Plans



View of Salisbury Street entrance



View of Gracefield Ave. entrance

RevID	CHD	Change Name	Date
01			24/02/2020

NOT FOR CONSTRUCTION

Do not scale.
The contractor shall verify all dimensions before commencing work, and all discrepancies reported to Field Ltd. for clarification.



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Client
The Youth Hub Trust

Project
Te Hurihanga Ō Rangatahi - The Youth Hub
109 Salisbury Street Christchurch New Zealand

Title
Indicative 3D Images

Scale
@ A1
Drawn
IN/JK
Checked
AK/AJ

Published Date: 27/02/2020 Issued for: Resource Consent

Project no.	Drawing no.	Rev.
18-008	A001	01



Over view of site from south east



Over view of site from north west

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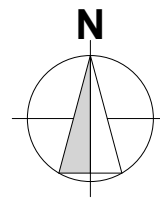
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Title
Indicative 3D Images

Scale
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 Drawn
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 AK/AJ

Published Date: 27/02/2020 Issued for: Resource Consent

Project no.	Drawing no.	Rev.
18-008	A002	01



North Elevation ▾

West Elevation - Part 1 ▾

South Elevation - Part 1 ▴

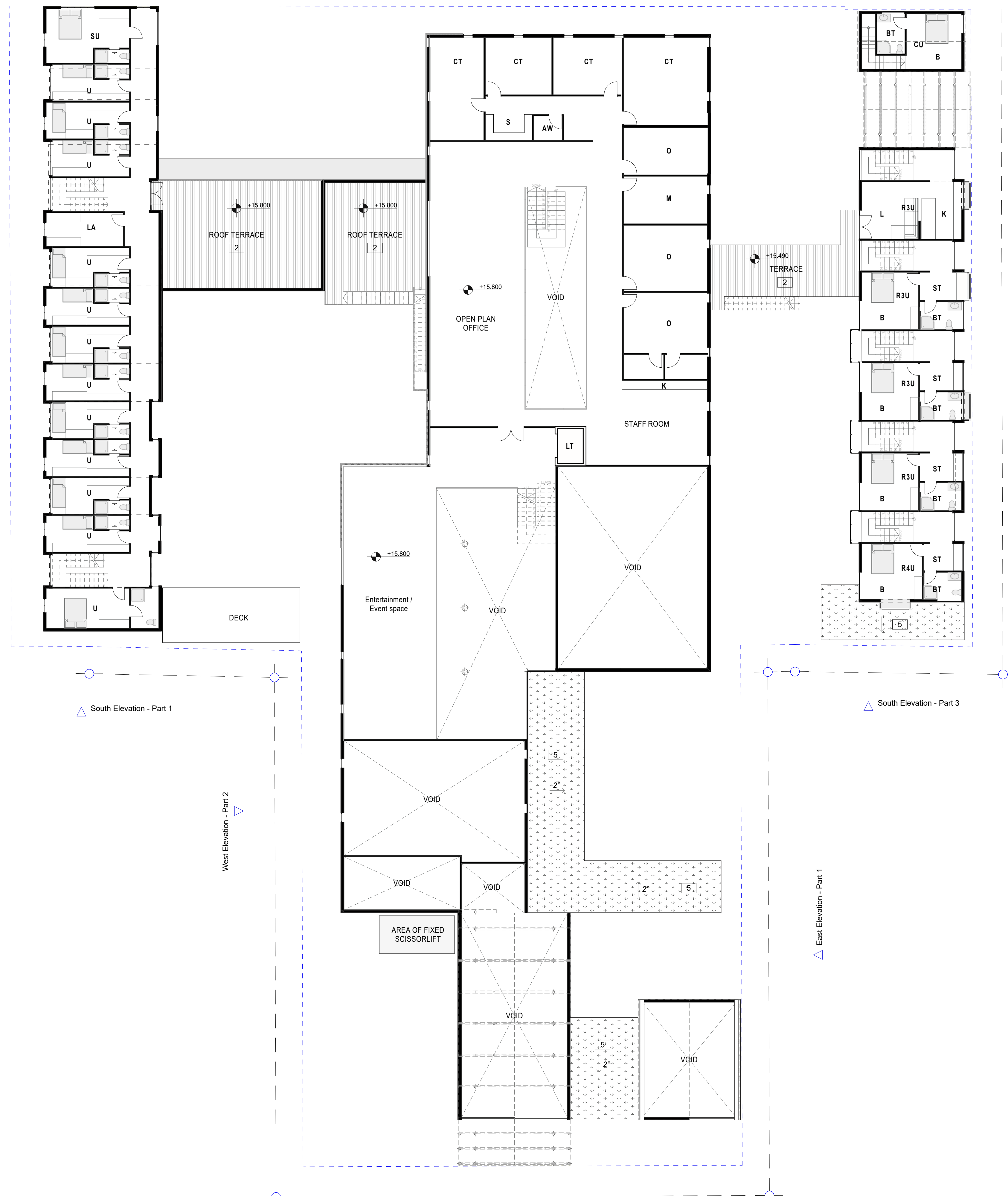
West Elevation - Part 2 ▾

South Elevation - Part 3 ▴

East Elevation - Part 1 ▴

South Elevation - Part 2 ▴

East Elevation ▴



ROOF PLAN LEGEND

- Roof Pitch
- 1 Greenhouse (glass) roofs
- 2 Walk-on terrace with membrane below
- 3a Long-run steel roofing (Kowhai Glo)
- 3b Long-run steel roofing (galvanised)
- 4 Membrane roof
- 5 Possible green roof with membrane below
- 6 Translucent profiled roofing
- 7 Timber Decking

FLOOR PLAN LEGEND

- Boundary
- Setback Line
- Roof Above/Below
- Void

SPACE DESIGNATIONS

- AW Accessible toilet
- AWS Accessible toilet with shower
- AU Accessible single sleeping unit with ensuite
- B Bed room
- BT Bathroom
- CT Consult room
- CU On-site caretaker unit
- D Dining
- K Kitchen
- KT Kitchenette
- L Living
- LA Laundry facilities
- LT Lift
- M Meeting
- O Office/Breakout
- PC Private courtyard
- R3U Residential 3 bedroom unit
- R4U Residential 4 bedroom unit
- S Storage
- ST Study
- SU Supervisor sleeping unit
- U Single sleeping unit with ensuite
- W Unisex toilet

RevID	CHD	Change Name	Date
01			24/02/2020

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109 Salisbury Street Christchurch New Zealand

Title: **First Floor Plan**

Scale: 1:150 @ A1
Drawn: IN/JK
Checked: AK/AJ

Project no: 18-008 Drawing no: A201 Rev: 01



1. Second Floor 1:150

RevID	CHD	Change Name	Date
01			24/02/2020

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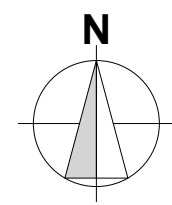
Client: The Youth Hub Trust

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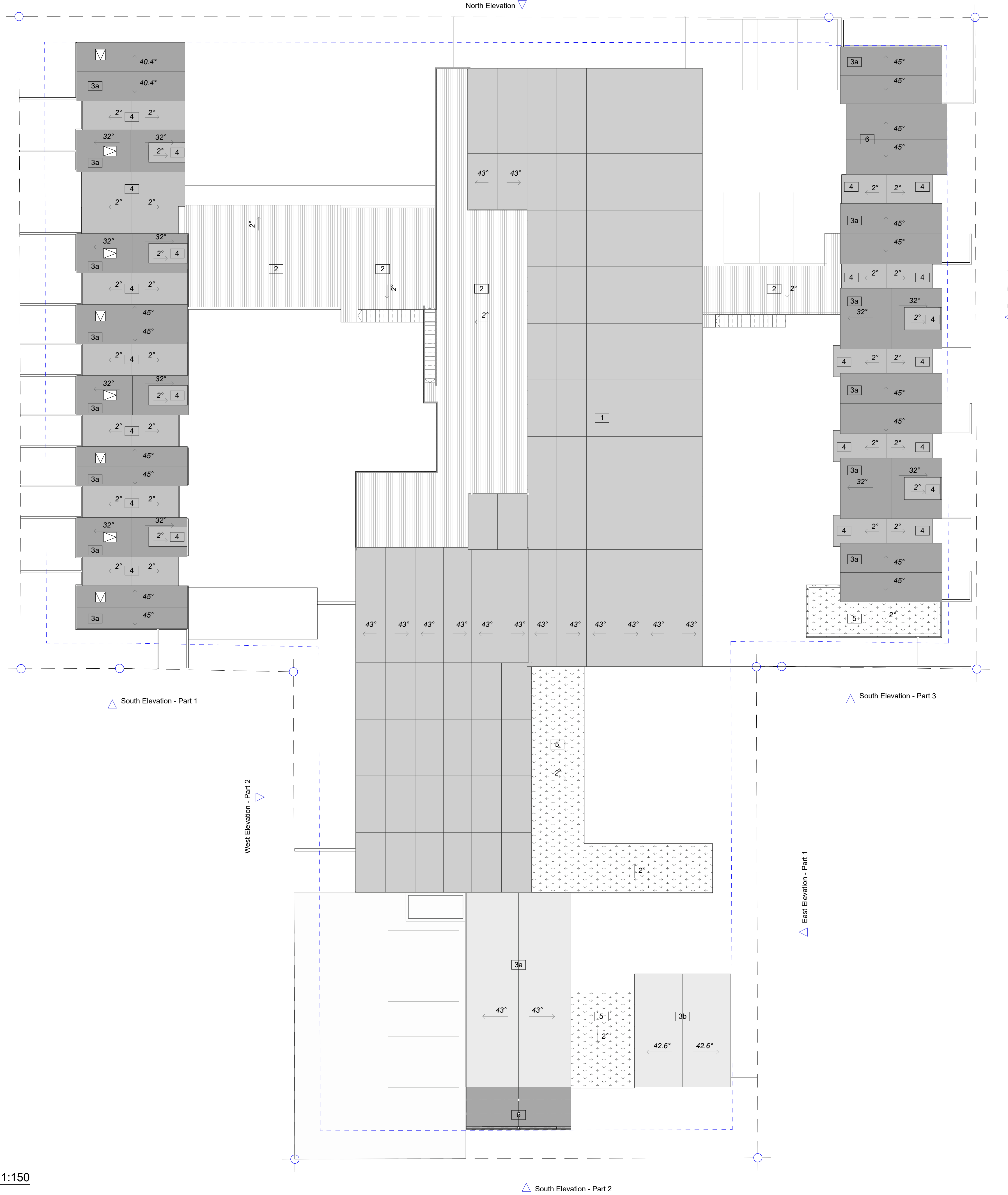
Title: **Second Floor Plan**

Scale: 1:150 @ A1
 Drawn: IN/JK
 Checked: AK/AJ

Project no.	Drawing no.	Rev.
18-008	A202	01



West Elevation - Part 1



ROOF PLAN LEGEND

Code	Description
→	Roof Pitch
1	Greenhouse (glass) roofs
2	Walk-on terrace with membrane below
3a	Long-run steel roofing (Kowhai Glo)
3b	Long-run steel roofing (galvanised)
4	Membrane roof
5	Possible green roof with membrane below
6	Translucent profiled roofing
7	Timber Decking

RevID	CHD	Change Name	Date
01			24/02/2020

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109 Salisbury Street Christchurch New Zealand

Title
Roof Plan

Scale
1:150 @ A1
Drawn
IN/JK
Checked
AK/AJ

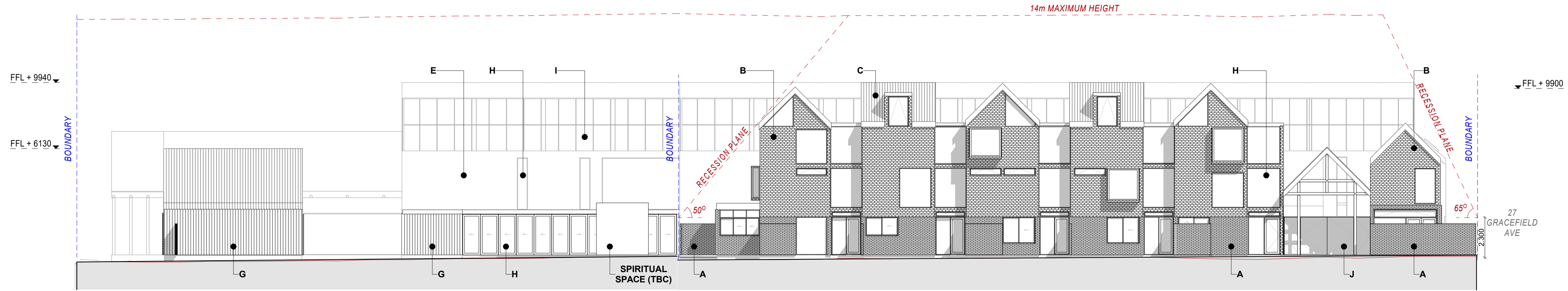
Project no.	Drawing no.	Rev.
18-008	A203	01

FINISHES LEGEND

Refer to 3D images on sheet A001 and A002 for indicative colour and materials

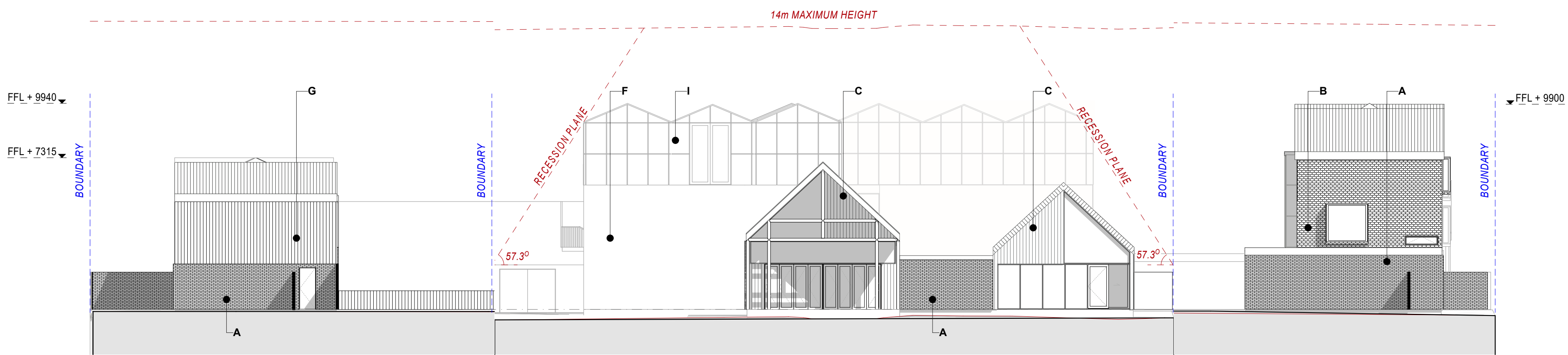
- A Brick (type & colour TBC)
- B Timber Shingles
- C Colorsteel Cladding in 'Kowhai Glo'
- D Galvanised Steel Cladding
- E Concrete with teal paint finish
- F Concrete with maroon paint finish
- G Vertical Timber Cladding
- H Glazing
- I Green House (Glazing)
- J Timber Gate

Existing ground levels
Proposed ground levels



1. East Elevation - Part 1 1:150
A101, A200, A201, A202, A203, A907

2. East Elevation - Gracefield Avenue 1:150
A101, A200, A201, A202, A203



3. South Elevation - Part 1 1:150
A101, A200, A201, A202, A203

4. South Elevation - Salisbury Street 1:150
A101, A200, A201, A202, A203

5. South Elevation - Part 3 1:150
A101, A200, A201, A202, A203

RevID	CHD	Change Name	Date
01			24/02/2020

NOT FOR CONSTRUCTION

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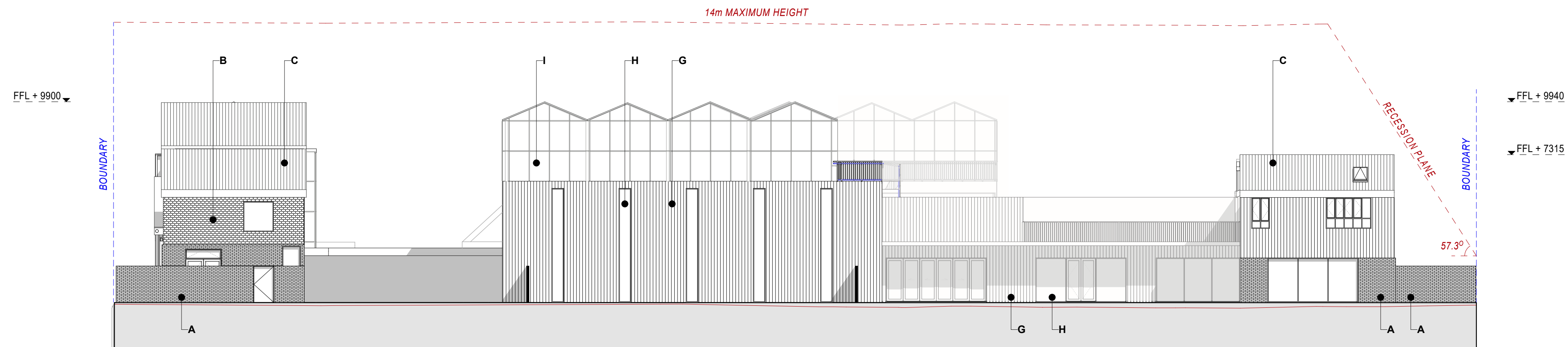
Project
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109 Salisbury Street Christchurch New Zealand

Title
Elevations

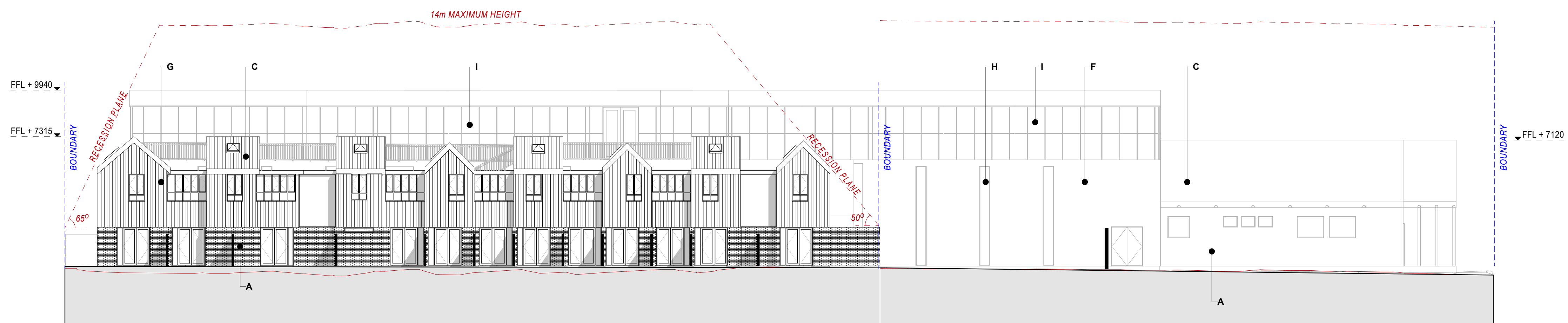
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Drawn
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AK/AJ

Publish Date: 27/02/2020
Issued for: Resource Consent

Project no.	Drawing no.	Rev.
18-008	A300	01



1. North Elevation 1:150
A101, A200, A201, A202, A203



2. West Elevation - Part 1 1:150
A101, A200, A201, A202, A203

3. West Elevation - Part 2 1:150
A101, A200, A201, A202, A203

FINISHES LEGEND

Refer to 3D images on sheet A001 and A002 for indicative colour and materials

- A Brick (type & colour TBC)
- B Timber Shingles
- C Colorsteel Cladding in 'Kowhai Glo'
- D Galvanised Steel Cladding
- E Concrete with teal paint finish
- F Concrete with maroon paint finish
- G Vertical Timber Cladding
- H Glazing
- I Green House (Glazing)
- J Timber Gate

Existing ground levels
Proposed ground levels

RevID	CHD	Change Name	Date
01			24/02/2020

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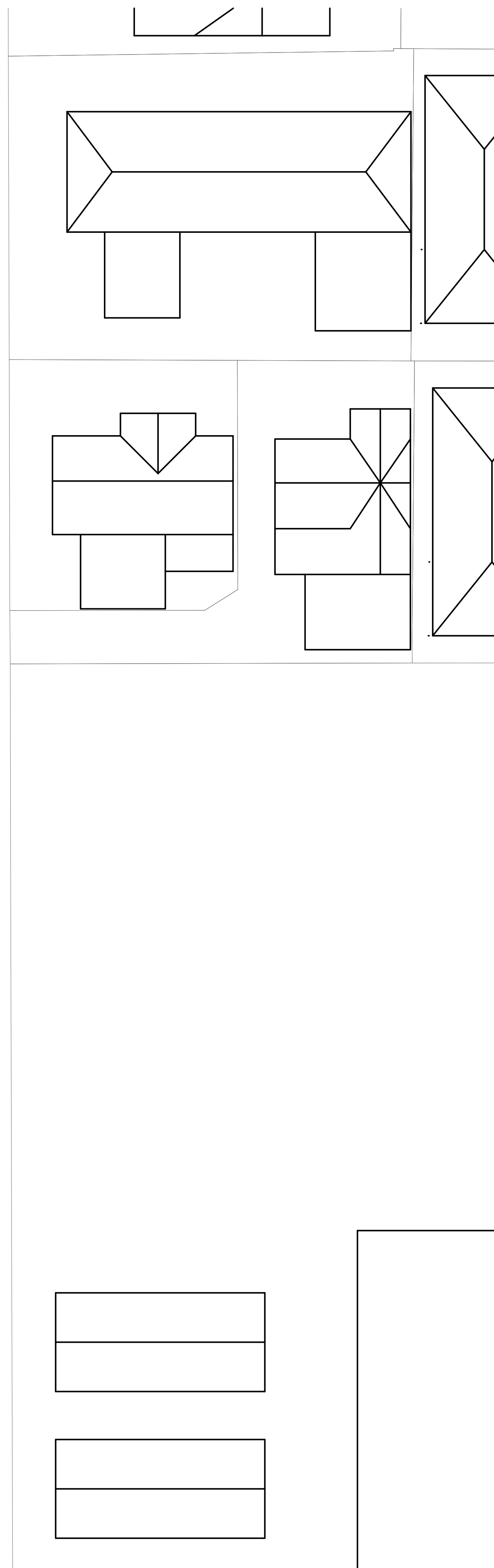
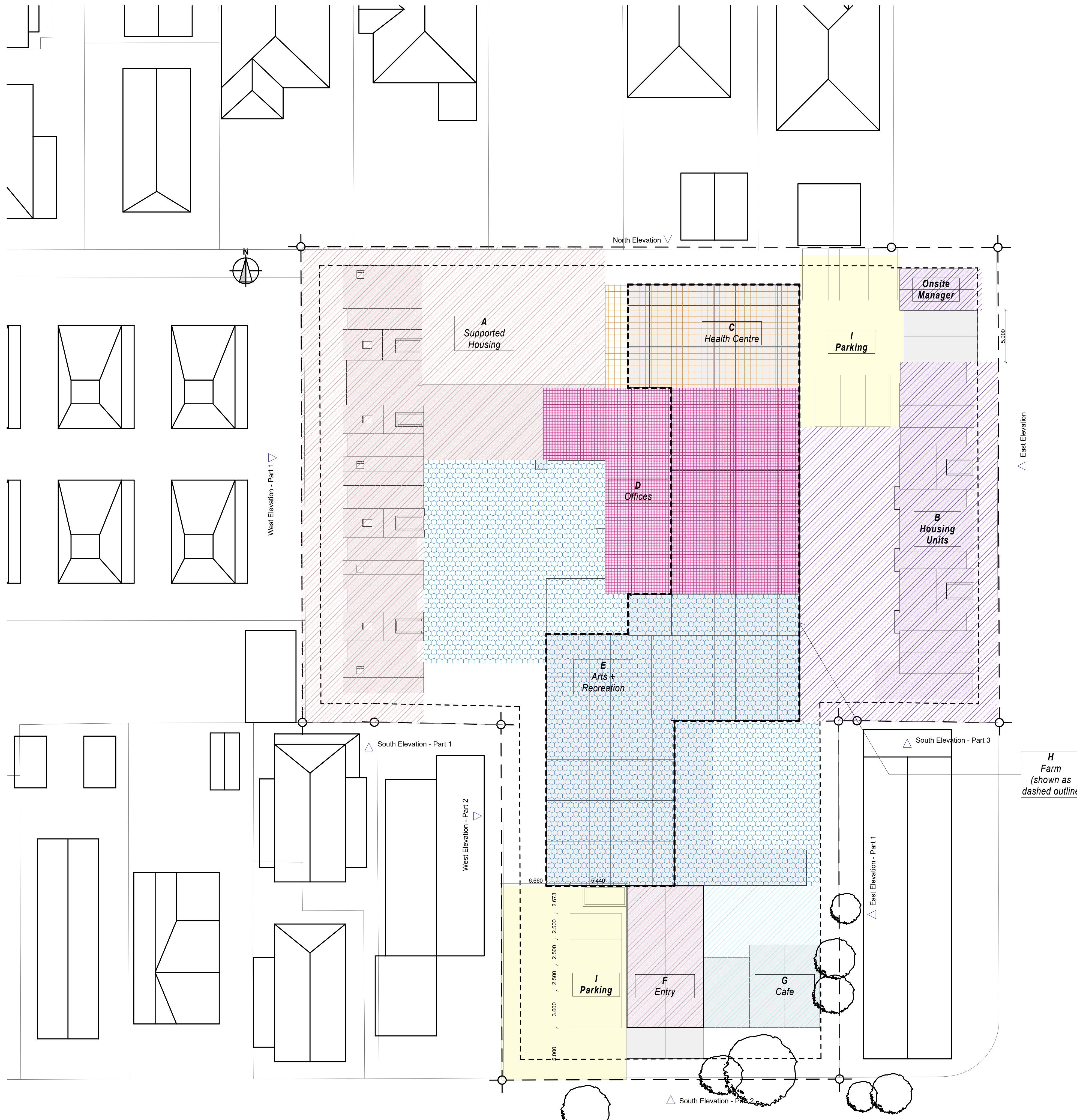
Title
Elevations

Scale
1:150 @ A1

Drawn
IN/JK

Checked
AK/AJ

Project no. 18-008
Drawing no. A301
Issued for Resource Consent
Rev. 01



PROJECT ZONES

- A Supported Housing
- B Gracefield Housing
- C Health Care
- D Offices
- E Arts + Recreation
- F Entry
- G Cafe
- H Farm
- I Parking

NOTE:

This drawing is intended to be read in colour



RevID	CHD	Change Name	Date
01			24/02/2020

NOT FOR CONSTRUCTION

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 Project
 Te Hurihanga Ō Rangatahi - The Youth Hub
 109 Salisbury Street Christchurch New Zealand

Project Zones

Scale
 1:200 @ A1
 Drawn
 IN/JK
 Checked
 AK/AJ

Published Date: 27/02/2020
 Issued for: Resource Consent

Project no.	Drawing no.	Rev.
18-008	A900	01



Appendix 2

Transport Compliance Assessment



RULE		COMMENT	COMPLIES?
7.4.3.1 Minimum number & dimensions of car parks required			
Within the Central City			
i). Any activity (except within the Residential Central City Zone)	Any car parking and associated manoeuvre area shall be no greater than 50% of the GLFA of the buildings on the site.	This standard is not applicable, as the site is within the Residential Central City zone. The area of parking is less than 50% of the GLFA.	N/A
ii). Any car parking spaces provided, except residential activities.	Any car parking spaces provided shall have the minimum dimensions in Appendix 7.5.1, Table 7.5.1.3.	The car parking complies with the required dimensions.	Yes
iii). Any activity (other than in respect of residential activities): A. where car parking spaces are provided, or B. containing buildings with GFA of more than 2,500m ²	The minimum number of mobility parking spaces in accordance with Appendix 7.5.1 shall be provided on the same site as the activity.	A minimum of one mobility car park is required and two are proposed.	Yes
7.4.3.2 Minimum number of cycle parking facilities required			
a). Any activity	At least the minimum amount of cycle parking facilities in accordance with Appendix 7.5.2 shall be provided on the same site as the activity.	The District Plan requires 23 visitor cycle parks plus 36 staff / resident cycle parks. 53 visitor cycle parks plus 42 resident / staff cycle parks are proposed. However, the layout of these cycle parks does not comply with the District Plan requirements.	No
7.4.3.3 Minimum number of loading spaces required			
a). Any activity where standard car parks are provided.	At least the minimum amount of loading spaces in accordance with Appendix 7.5.3 shall be provided on the same site as the activity.	No loading is required.	Yes
7.4.3.4 Manoeuvring for parking & loading areas			
a). Any activity with a vehicle access	On-site manoeuvring area shall be provided in accordance with Appendix 7.5.6.	Additional manoeuvring is required to enter and exit parking spaces accessed via Gracefield Avenue.	No
b). Any activity with a vehicle access to: i. a major or minor arterial road; or	On-site manoeuvring area shall be provided to ensure that a vehicle can manoeuvre in a forward gear on to and off a site.	The site has all vehicles entering and exiting forwards.	Yes



RULE		COMMENT	COMPLIES?
<ul style="list-style-type: none"> ii. a collector road where three or more car parking spaces are provided on site; or iii. six or more car parking spaces; or iv. a heavy vehicle bay required by Rule 7.4.3.3; or v. a local street or local distributor street within the Central City core; or vi. a main distributor street within the Central City where the vehicle access serves three or more parking spaces; or vii. a local street outside the Central City core and the vehicle access serves six or more parking spaces. 			
7.4.3.5 Gradient of parking and loading areas			
a). All non-residential activities with vehicle access.	Gradient of surfaces at 90 degrees to the angle of parking (i.e. parking stall width) - Gradient shall be $\leq 1:16$ (6.26%)	Complies	Yes
b). All non-residential activities with vehicle access	Gradient of surfaces parallel to the angle of parking (i.e. parking stall length). - Gradient shall be $\leq 1:20$ (5%)	Complies	Yes
c). All non-residential activities with vehicle access	Gradient of mobility car park spaces - Gradient shall be $\leq 1:50$ (2%)	Complies	Yes
7.4.3.6 Design of parking and loading areas			
a). All non-residential activities with parking and/or loading areas used during hours of darkness.	Lighting of parking and loading areas shall be maintained at a minimum level of two lux, with high uniformity, during the hours of operation.	Whilst detailed design has not been undertaken, it is anticipated that the site will comply.	Yes
b). Any urban activity, except: <ul style="list-style-type: none"> i. residential activities containing less than three car parking spaces; or ii. sites where access is obtained from an unsealed road; or iii. temporary activities. 	The surface of all car parking, loading, and associated access areas shall be formed, sealed and drained and car parking spaces permanently marked.	The surface will be formed, sealed and drained with parking spaces permanently marked.	Yes



RULE		COMMENT	COMPLIES?
7.4.3.7 Access design			
a). Any activity with vehicle access.	Access shall be provided in accordance with Appendix 7.5.7.	The Gracefield Avenue access does not provide the required pedestrian visibility splay.	No
b). Any activity providing 4 or more car parking spaces or residential units.	Queuing spaces shall be provided in accordance with Appendix 7.5.8.	No queue space is required at the accesses	Yes
c). Outside the Central City, any vehicle access: i. to an urban road serving more than 15 car parking spaces or more than 10 heavy vehicle movements per day; and/or ii. on a key pedestrian frontage	Either an audio and visual method of warning pedestrians of the presence of vehicles or a visibility splay in accordance with Appendix 7.5.9 shall be provided. If any part of the access lies within 20 metres of a Residential Zone any audio method should not operate between 20:00 and 08:00 hours.	Not applicable	N/A
d). Within the Central City, any vehicle access to a road serving more than 15 car parking spaces or more than 10 heavy vehicle movements per day, where the site provides access onto any street within the core.	An audio and visual method of warning pedestrians of the presence of vehicles about to exit the access point shall be provided.	Not applicable	N/A
e). Within the Central City, any vehicle access to a road serving more than 15 car parking spaces or more than 10 heavy vehicle movements per day, in any other location not covered by clause d above.	Either an audio and visual method of warning pedestrians of the presence of vehicles about to exit the access point or a visibility splay in accordance with Appendix 7.5.9 – Visibility splay, shall be provided. If any part of the access lies within 20 metres of a Residential Central City Zone any audio method should not operate between 20:00 and 08:00 hours, except when associated with an emergency service vehicle.	Not applicable	N/A
7.4.3.8 Vehicle crossings			
a). Any activity with a vehicle access to any road or service lane.	A vehicle crossing shall be provided constructed from the property boundary to the edge of the carriageway / service lane.	Complies	Yes
b). Any vehicle crossing on an arterial road or collector road with a speed limit 70 kilometres per hour or greater.	Vehicle crossing shall be provided in accordance with Appendix 7.10.	Not applicable	N/A
c). Any vehicle crossing to a rural selling place.	Vehicle crossing shall be provided in accordance with Figure 14 in Appendix 7.5.10.	Not applicable	N/A



RULE		COMMENT	COMPLIES?
d). Any vehicle crossing on a road with a speed limit 70 kilometres per hour or greater	The minimum spacing to an adjacent vehicle crossing on the same side of the frontage road, shall be in accordance with Table 7.5.11.1 in Appendix 7.5.11.	Not applicable	N/A
e). Any activity with a vehicle crossing	The maximum number of vehicle crossings shall be in accordance with Table 7.5.11.2 (outside the Central City) and Table 7.5.11.3 (within the Central City) in Appendix 7.5.11.	There is only one crossing per frontage, so the proposal complies.	Yes
f). Any activity with a vehicle crossing	The minimum distance between a vehicle crossing and an intersection shall be in accordance with the Table 7.5.11.4 (outside the Central City) and Table 7.5.11.5 (within the Central City) in Appendix 7.5.11.	Complies	Yes
g). Any vehicle crossing on a rural road	The minimum sight lines to vehicle crossings shall be provided in accordance with Figure 18 in Appendix 7.5.11.	Not applicable	N/A
7.4.3.9 Location of buildings and access in relation to road/rail level crossings			
a). Any new road or access that crosses a railway line	No new road or access shall cross a railway line.	Not applicable	N/A
b). All new road intersections located less than 30 metres from a rail level crossing limit line	The road intersection shall be designed to give priority to rail movements at the level crossing through road traffic signals.	Not applicable	N/A
c). All new vehicle crossings located less than 30 metres from a rail level crossing limit line.	No new vehicle crossing shall be located less than 30 metres from a rail level crossing limit line unless the boundaries of a site do not enable the vehicle crossing to be more than 30 metres from a rail level crossing limit line.	Not applicable	N/A
d). Any building located close to a level crossing not controlled by automated warning devices (such as alarms and/or barrier arms).	Buildings shall be located outside of the sight triangles in Appendix 7.5.13.	Not applicable	N/A
7.4.3.10 High trip generators			



RULE		COMMENT	COMPLIES?
k. Mixed Use	More than 50 vehicle movements in the PM peak is a controlled activity and more than 120 vehicle movements is Restricted Discretionary.	The proposed activity has been estimated as generating 34 to 83 vehicle movements per hour, so assessment is required against the Basic ITA matters.	No
7.4.3.11 Vehicle access to sites fronting more than one street – Within the Central City			
a). Any new vehicle access.	Vehicle access shall be provided in accordance with Appendix 7.5.15.	Access should only be taken from Salisbury Street, whereas access is also proposed to Gracefield Avenue.	No
7.4.3.12 Lane Formation – Within the Central City			
a). Any new Central City lane created.	The legal width of the Central City lane shall be between 6m and 12m and have a minimum height clearance of 4.5m.	Not applicable	N/A



	CAR PARKS		CYCLE PARKS		LOADING	
	Residents / Visitors	Staff	Visitors	Staff / Resident	HGV	99% Car
Residential (6 units)	<i>1 per unit</i>	<i>Nil</i>	<i>1 / 20 units</i>	<i>1 per unit without a garage</i>	<i>Nil</i>	<i>Nil</i>
	6	-	0.3	6	-	-
Sheltered Housing (24 units, including 2 staff units)	<i>1 per 4 units</i>	<i>1 per resident / staff unit</i>	<i>1 / 20 units</i>	<i>1 per unit without a garage</i>	<i>Nil</i>	<i>Nil</i>
	5.5+	2	1.2	24	-	-
Food & Beverage (119m ² PFA)	<i>9/100m² PFA</i>	<i>1/100m² PFA (2 min)</i>	<i>1/125m² PFA</i>	<i>1/400m² PFA</i>	<i>1/1,000m² PFA</i>	<i>Nil</i>
	10.7	1.2	1.0	0.3	0.1	-
Entertainment (737m ² PFA)	<i>1/10m² PFA</i>	<i>10% of visitor requirement</i>	<i>1/50m² PFA</i>	<i>10% of visitor requirement</i>	<i>Nil</i>	<i>1/2,000m² PFA</i>
	73.7	7.4	14.7	1.5	-	0.4
Healthcare Facility (231m ² GFA)	<i>1/25m² GFA</i>	<i>1/100m² GFA</i>	<i>1/500m² GFA</i>	<i>1/300m² GFA</i>	<i>Nil</i>	<i>Nil</i>
	9.2	2.3	0.5	0.8	-	-
Office (419m ² GFA)	<i>5% of visitor requirement</i>	<i>2.5/100m²</i>	<i>1/500m² GFA</i>	<i>1/150m² GFA</i>	<i>1/8,000m²</i>	<i>1/8,000m²</i>
	0.5	10.5	0.8	2.8	0.0	0.0
Basketball Court (133m ²)	<i>1/50m² court area</i>	<i>1/200m² court area</i>	<i>1/150m² court area</i>	<i>1/500m² PFA</i>	<i>Nil</i>	<i>Nil</i>
	2.7	0.7	0.9	0.3	-	-
Market Garden (191m ²)	<i>1 / 10m² PFA</i>	<i>10% of visitor requirements</i>	<i>1 / 50m² PFA</i>	<i>10% of visitor requirements</i>	<i>Nil</i>	<i>1/2,000m² PFA</i>



	19.1	1.9	3.8	0.4	-	0.1
Sub-Total	127.4	26	23	36	0	0
Reduced Total	59	12	See following for calculation of Reduction Factor of 54%			

	FACTOR	DESCRIPTION	REDUCTION FROM THE MINIMUM PARKING REQUIREMENT	ASSESSMENT	RESULT		
Permitted reductions (without the need for a resource consent)	Public transport accessibility	Located within a 400m walk of a public transport stop served by a public transport service ¹ with a frequency of at least 15 minutes on weekdays between 7am and 6pm.	Between 0m and 100m: 10% reduction per service.	Within 25m of the Blue Line	10%		
			Between 101m and 200m: 6% reduction per service.				
			Between 201m and 400m: 3% reduction per service.				
			Up to a maximum of 16%.				
			Between 0m and 50m: 5% reduction per service.				
			Up to a maximum of 8%				
	Public parking facility	Located within a 200m walk of a public transport stop served by a public transport service with a frequency of at least 30 minutes on weekdays between 7am and 6pm.	Between 51m and 125m: 3% reduction per service.	Within 25m of Route 29. Within 125m of Route 28	8%		
			Between 126m and 200m: 1% reduction per service.				
			Up to a maximum of 8%				
			Between 0m and 50m: 10% reduction.			Within 134m of the parking at Peterborough Street / Durham Street	6%
			Between 51m and 200m: 6% reduction.				
			Between 201m and 400m: 2% reduction.				
Walking accessibility	Located within a 400m walk of an identified commercial core zone (refer to Chapter 15):	Between 0m and 50m: 15% reduction.	184m from Commercial Central City Business	10%			
		Between 51m and 200m: 10% reduction.					



	FACTOR	DESCRIPTION	REDUCTION FROM THE MINIMUM PARKING REQUIREMENT	ASSESSMENT	RESULT
			Between 201m and 400m: 5% reduction.		
	e	Access to a Major Cycle Route Located within 1.2km of a Major Cycle Route	Between 0m and 150m: 15% reduction. Between 151m and 600m: 10% reduction. Between 601m and 1,200m: 5% reduction.	Within 150m of MCR on Colombo Street	15%
	f	Cycle parking The number of cycle parks (and lockers and showers) provided for the activity exceeds the requirements under 7.2.3.2 Rule 2 (cycle parking requirements).	Cycle parking exceeds requirements by 5% to 10%: 5% reduction. Cycle parking exceeds requirements by more than 10%: 10% reduction.		
Reductions based on assessment through the resource consent process	g	Mixed-use development Developments that contain a mix of both residential activities and activities where people are employed at the site	Up to 5%	Yes	5%
	h	Good non-vehicular access to buildings There is a pedestrian access way that: - is separated from the vehicle access and parking areas, - has a direct distance of less than 10m from a footpath on public road reserve to the activity's main building public entrance2	Up to 3%		
	i	Integration with public transport Activities that include a dedicated indoor waiting area for users of public transport or taxis that is safe, sheltered, attractive, accessible, and comfortable	Up to 5%		
	j	Travel plan The activity provides a travel plan that: - Includes measures to encourage public transport use - Includes measures to encourage walking and cycling	Up to 10%		



FACTOR	DESCRIPTION	REDUCTION FROM THE MINIMUM PARKING REQUIREMENT	ASSESSMENT	RESULT
	- Includes ways to make travel by the private car more efficient (such as through car-pooling)			
	- Sets out a contingency arrangement in case of overflow car parking			
	- Describes the ways in which the travel plan will be implemented			
	- Includes ways to monitor the effectiveness of the travel plan			
	- Includes enforcement measures			

Permitted = 49%

Via Consent = 5%

Total = 54%



Appendix 3

Parking Demand by Time

Residential

	Residential	Staff	Visitor
	85%	15%	0
0:00	4	1	0
1:00	4	1	0
2:00	4	1	0
3:00	4	1	0
4:00	4	1	0
5:00	4	1	0
6:00	4	1	0
7:00	4	1	0
8:00	4	1	0
9:00	4	1	0
10:00	4	1	0
11:00	4	1	0
12:00	4	1	0
13:00	4	1	0
14:00	4	1	0
15:00	4	1	0
16:00	4	1	0
17:00	4	1	0
18:00	4	1	0
19:00	4	1	0
20:00	4	1	0
21:00	4	1	0
22:00	4	1	0
23:00	4	1	0

Youthline

	Residential	Staff	Visitor
	0%	20%	80%
0:00	0	0	0
1:00	0	0	0
2:00	0	0	0
3:00	0	0	0
4:00	0	0	0
5:00	0	0	0
6:00	0	0	0
7:00	0	0	0
8:00	0	1	5
9:00	0	1	5
10:00	0	1	5
11:00	0	1	5
12:00	0	1	5
13:00	0	1	5
14:00	0	1	5
15:00	0	1	5
16:00	0	1	5
17:00	0	1	5
18:00	0	1	5
19:00	0	1	5
20:00	0	1	5
21:00	0	1	5
22:00	0	1	0
23:00	0	1	0

Café

	Residential Staff		Visitor
	0%	10%	90%
0:00	0	0	0
1:00	0	0	0
2:00	0	0	0
3:00	0	0	0
4:00	0	0	0
5:00	0	0	0
6:00	0	0	0
7:00	0	0	0
8:00	0	0	0
9:00	0	0	4
10:00	0	0	4
11:00	0	0	4
12:00	0	0	4
13:00	0	0	4
14:00	0	0	4
15:00	0	0	4
16:00	0	0	4
17:00	0	0	0
18:00	0	0	0
19:00	0	0	0
20:00	0	0	0
21:00	0	0	0
22:00	0	0	0
23:00	0	0	0

Market Garden

	Residential Staff		Visitor
0:00	0	0	0
1:00	0	0	0
2:00	0	0	0
3:00	0	0	0
4:00	0	0	0
5:00	0	0	0
6:00	0	0	0
7:00	0	0	0
8:00	0	4	7
9:00	0	4	7
10:00	0	4	7
11:00	0	4	7
12:00	0	4	7
13:00	0	4	7
14:00	0	4	7
15:00	0	4	7
16:00	0	4	7
17:00	0	0	0
18:00	0	0	0
19:00	0	0	0
20:00	0	0	0
21:00	0	0	0
22:00	0	0	0
23:00	0	0	0

Event / Recreation Space

	Residential Staff		Visitor
	0%	9%	91%
0:00	0	0	0
1:00	0	0	0
2:00	0	0	0
3:00	0	0	0
4:00	0	0	0
5:00	0	0	0
6:00	0	0	0
7:00	0	0	0
8:00	0	0	0
9:00	0	2	16
10:00	0	2	16
11:00	0	2	16
12:00	0	2	16
13:00	0	2	16
14:00	0	2	16
15:00	0	2	16
16:00	0	2	16
17:00	0	2	16
18:00	0	2	16
19:00	0	2	16
20:00	0	2	16
21:00	0	2	16
22:00	0	2	16
23:00	0	0	0

Other Agencies / Office

	Residential Staff		Visitor
	0%	95%	5%
0:00	0	0	0
1:00	0	0	0
2:00	0	0	0
3:00	0	0	0
4:00	0	0	0
5:00	0	0	0
6:00	0	0	0
7:00	0	5	0
8:00	0	5	0
9:00	0	5	0
10:00	0	5	0
11:00	0	5	0
12:00	0	5	0
13:00	0	5	0
14:00	0	5	0
15:00	0	5	0
16:00	0	5	0
17:00	0	5	0
18:00	0	0	0
19:00	0	0	0
20:00	0	0	0
21:00	0	0	0
22:00	0	0	0
23:00	0	0	0

Summary

	Residential	Staff	Visitor	Total
0:00	4	1	0	5
1:00	4	1	0	5
2:00	4	1	0	5
3:00	4	1	0	5
4:00	4	1	0	5
5:00	4	1	0	5
6:00	4	1	0	5
7:00	4	6	0	10
8:00	4	11	12	27
9:00	4	12	33	49
10:00	4	12	33	49
11:00	4	12	33	49
12:00	4	12	33	49
13:00	4	12	33	49
14:00	4	12	33	49
15:00	4	12	33	49
16:00	4	12	33	49
17:00	4	8	21	34
18:00	4	4	21	29
19:00	4	4	21	29
20:00	4	4	21	29
21:00	4	4	21	29
22:00	4	4	16	24
23:00	4	2	0	6



Appendix 4

Travel Management Plan Framework



Travel Management Measures – Draft Framework Only

Introduction

The purpose of this section is to outline the measures proposed for the Travel Management Plan associated with the Youth Hub Development at 109 Salisbury Street, Christchurch. The intention of this Plan is that it is adaptive and forms an initial step in the travel management process for the site.

The following is a template of the measures that could be included within the Travel Management Plan. This Plan will need to be fully developed prior to opening of the activity.

Objectives

The objectives of the Travel Management Plan are as follows:

- Encourage travel to / from the site by modes other than private car travel as far as is practicable; and
- Manage the car parking effects associated with those people that choose to travel to the site.

It is envisioned that this Travel Management Plan will be reviewed on a regular basis to confirm that it remains effective and relevant. It is suggested that it be reviewed within six months of the activities commencing, and then yearly to confirm it remains fit for purpose.

Travel Management Measures

Overall Travel Management Measures

Travel Management Co-ordinator

A Travel Management Co-ordinator will be appointed for the site. This person will be responsible for the implementation and ongoing reviews of the Travel Management Plan at the site. They will also be responsible for ensuring travel information is up to date and be a point of contact for staff regarding potential improvements to the Plan.

Website Information

It is anticipated there will be webpages for the site and potentially webpages for the individual organisations located at the site. The webpage will include “getting here” information, which will include the information identified below.

Car Travel

Although the intention of the Travel Management Plan is to minimise travel by private car, it is accepted that car travel remains one of the primary transport modes within Christchurch. As such, the following sets out measures to manage car parking at the site.

Staff Car-Pooling

It is anticipated that the number of staff at the application site will be too small to warrant use of formal car-pooling software. However, it is proposed that an informal car-pooling system be instigated. This could be simply provided via a sign-up system on a bullet-in board for people who are interested in car-pooling to be able to contact each other.



Staff / Resident Car Parking Locations

The site has a limited amount of staff car parking, which is assumed to be allocated (rather than on a first come first served basis). Prior to relocating to the application site, staff will be made aware of the limited availability of this car parking and advised they will need to park elsewhere. It is advised that staff be discouraged from parking on Gracefield Avenue through this process because of the sensitivity regarding traffic on this road.

Staff will be advised of the local off-street car parking options, including the availability of parking at the Casino site. Staff can also be advised of other off-street car parking options in the vicinity of the site, should these become available.

Residents will be advised of the lack of on-site parking when they arrive, although this is anticipated to be less of an issue than staff because of the age of the residents.

Visitor Car Parking

The *Getting Here* section of the webpage will advise people there is very limited car parking at the site and to consider alternate locations. This will include a reference to the availability of the Casino car park (ideally including a link to the pricing structure). A section will also be included to request that visitors do not park on Gracefield Avenue.

It is anticipated that the visitor car parking at the site may need to be dedicated for pick-up / drop-off only for certain events in the *Event Space*. It is proposed that temporary P5 signage be placed at the Salisbury Street access prior to events that may incur a notable number of pick-up / drop-off trips (to be at the discretion of the Travel Management Plan Co-ordinator in conjunction with the event holder). The use of the visitor car parks will be monitored / managed during events to ensure they are only being used for pick-up / drop-off and to avoid congestion occurring on Salisbury Street. This is anticipated to relate to on-site management only, with no management occurring on-street.

Subsequent to gaining resource consent, it is recommended that the applicant liaise with Council to alter the time restrictions on Salisbury Street along the site frontage with the intention of facilitating further visitor car parking for pick-up / drop-off activities. It is understood Council will only enter these discussions post granting of the resource consent and that it will require Community Board approval (potentially with community consultation).

Cycle Travel

Staff / Residents

The site provides a more than compliant supply of staff and resident cycle parking. The use of this cycle parking will be monitored and feedback sought from the staff to determine whether the supply is sufficient to accommodate the demand, or whether a lack of supply is perceived to be a barrier to cycling. Best endeavours will be made to provide additional cycle parking should the need arise, although it is accepted there is limited area to provide covered and secure cycle parking at the site.

The site will provide lockers for staff cycling to work and staff showers are also proposed.

It is recommended that the site includes a puncture repair kit to facilitate minor repairs. It is also recommended that the site includes an iron and ironing board at a shared location.



Visitors

The *Getting Here* website page will include links to the Council's cycle maps. Physical copies of the cycle network maps will be kept at the site for visitors to take.

Sufficient visitor cycle parking is proposed at the site to meet the District Plan requirements. The use of this cycle parking shall be informally monitored to confirm that it is sufficient to meet the demands. Best endeavours will be made to provide additional visitor cycle parking should the need arise.

Walking / Running

The site will include showers and ironing facilities for people that choose to walk or run to work. It is also recommended that the site provides umbrellas for staff use during the day, or to take home for the night (if required).

Passenger Transport

The *Getting Here* website page will include a link to the online bus route and timetable page to encourage use of public transport. Copies of timetables for the most relevant bus services for the site will be made available for staff and visitors (currently the Blue Line, Routes 28, 29 and 95).

Micro Transport

The site is to include storage facilities for micro-transport (such as skateboards and scooters). Limited charging for e-transport can be considered, so long as this is practical and functional.

Loading

Whilst not typically considered as part of a Transport Management Plan, car parks in the visitor parking area (accessed via Salisbury Street) will need to account for loading at the site.

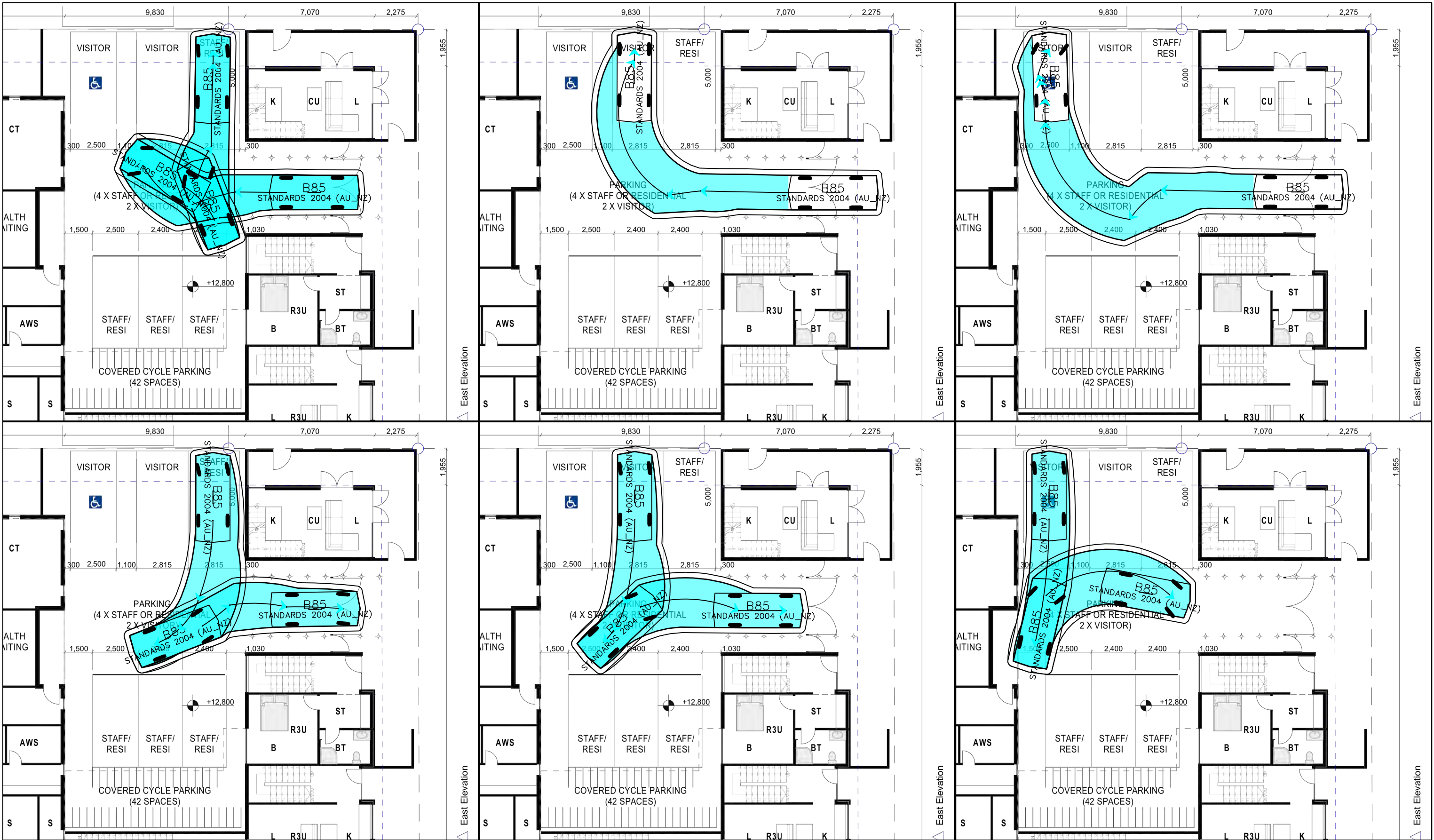
The majority of loading is anticipated to occur via small courier vehicles that will be able to use the visitor car parking. Events at the site may require prolonged use of the visitor car park for the unloading / loading of equipment. The car park can be coned off for this use for short durations, although it is recommended these vehicles then vacate the site so this area can accommodate pick-up / drop-off vehicles for the event.

Occasional deliveries will also occur for the community garden, which will require the coning off of the end space. This occasional use will need to be co-ordinated on-site and the space made available, most likely by coning off this space early on the day of the deliveries.



Appendix 5

Vehicle Tracking



Novo Group Limited
PO Box 365
Christchurch 8014

NovoGroup.co.nz

Te Hurihanga ō Rangatahi – The Youth Hub 109 Salisbury Street, Christchurch

Gracefield Avenue - 85th Percentile Vehicle Tracking

For Resource Consent

619-001 - TR003 Rev B

Sheet

T01A

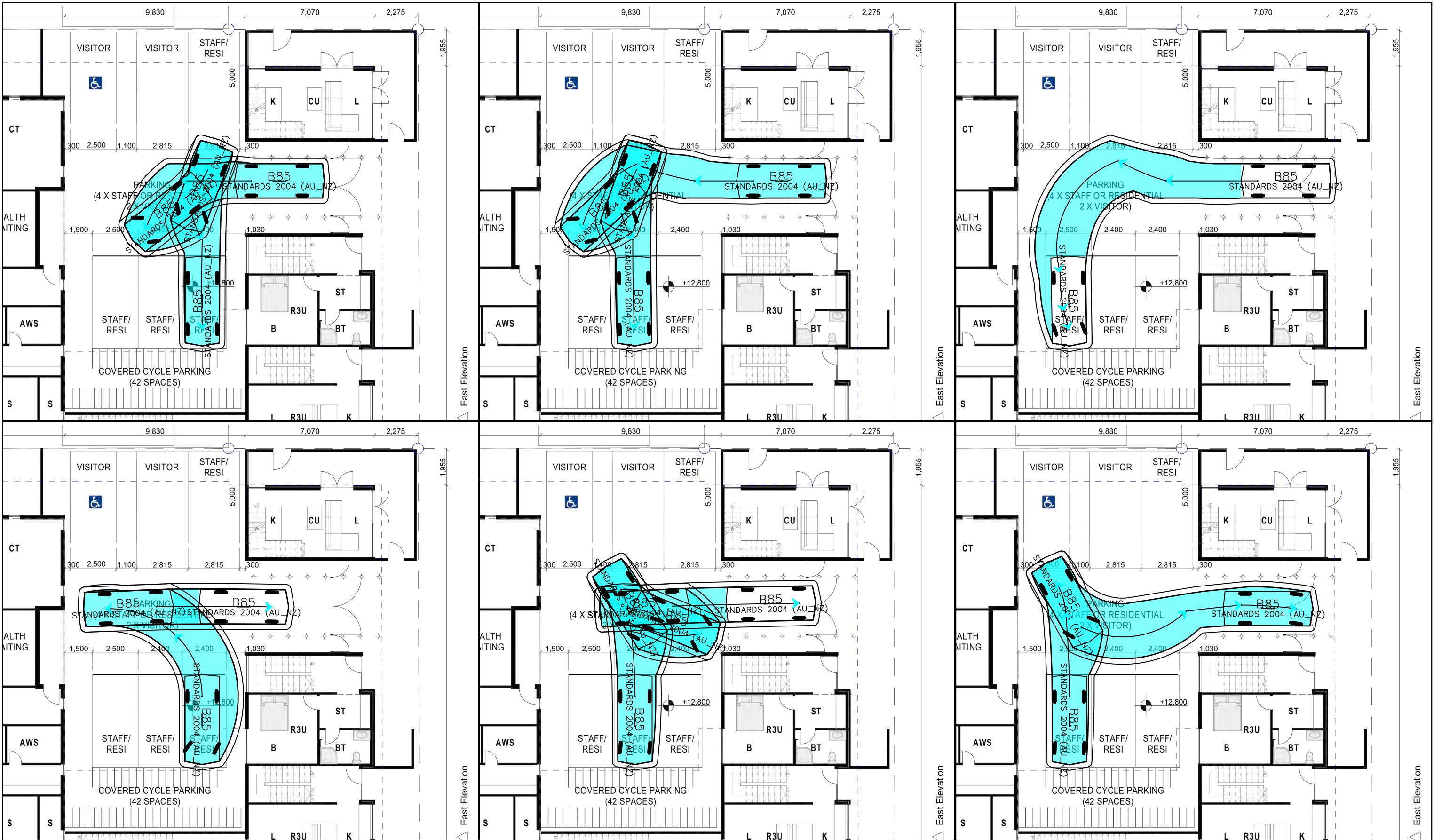
Scale @A3

Date 11/02/2020

By N Fuller

Project # 619-001

Drawing:



Novo Group Limited
PO Box 365
Christchurch 8014

NovoGroup.co.nz

Te Hurihanga ō Rangatahi – The Youth Hub 109 Salisbury Street, Christchurch

Gracefield Avenue - 85th Percentile Vehicle Tracking

For Resource Consent

619-001 - TR003 Rev B

Sheet

T02A

Scale @A3

Date 11/02/2020

By N Fuller

Project # 619-001

Drawing:

Appendix 8

Urban Design Assessment

27 February 2020

MEMO

TO: Kim Seaton, Novo Group
FROM: Anne Wilkins, Principal Urban Designer / Landscape Architect
PROJECT REF: 619-001

YOUTH HUB CHRISTCHURCH - URBAN DESIGN ASSESSMENT

1. The following details the Urban Design Assessment for the Youth Hub project proposal. The assessment is to review whether the proposal aligns with urban design considerations, such as streetscape, activation, built form, and aesthetic quality, alongside the potential for integration and association with the wider urban environment.
2. Supporting information for the assessment included the:
 - i) Architectural Design Plans, by Field Architecture dated 27.02.20
 - ii) Traffic Report, by Novo Group dated 27.02.20.
 - iii) Landscape Design Plans by Wood Landscape Architecture dated 02.2020

Context and Analysis

3. The landscape is a mixed residential and commercial area. The residential area to the north is made up of townhouses, bungalows and higher density townhouse buildings. The commercial area is varied, with offices across Salisbury St, the Salvation Army building across Gracefield Ave and the mixed retail at Peterborough St. There are also a number of motels and accommodation facilities in and around the area.
4. The site is within a fringe area, where the commercial area merges with residential. Growth, urban spread and land change situated inside the four avenues means a highly dynamic and adapting environment surrounding the area. The site is located short of 100 metres from the busy retail and commercial area, housing the Christchurch Casino, shops, cafes and restaurants.
5. There are a number of recreational areas in the vicinity. Hagley Park lies to the southwest of the site, a large recreational space characteristic to the city. The Avon River traverses to the south, typically used for walking, cycling or recreational uses. The small pocket park Cranmer Square is to the southwest. The existing network of these recreational spaces will act to link the potential Youth Hub site to the wider urban fabric.



6. The relevant urban design provisions and policies are listed in the Christchurch City District Plan (CCDP) in the **Residential Central City Zone, Design Principles 14.5.33**. These are outlined and assessed below.

14.15.33	a) <i>In extent to which the development, while bringing change to existing environments:</i>
Urban Design in the Residential Central City Zone	i) <i>Engages with and contributes to adjacent streets, lanes and public open spaces.</i>
	ii) <i>integrates access, parking areas and garages in a way that is safe for pedestrians and cyclists, and that does not dominate the development</i>
	iii) <i>has appropriate regard to:</i> A. <i>residential amenity for occupants, neighbours and the public, in respect of outlook, privacy, and incorporation of Crime Prevention Through Environmental Design principles; and;</i> B. <i>neighbourhood context, existing design styles and established landscape features on the site or adjacent sites;</i>
	iv) <i>provides for human scale and creates sufficient visual quality and interest</i>

Streetscape: Policy 14.15.33a (i) and (iv)

Regarding the current outlook of the street and infrastructure, and the existing trees to remain by the new carparking entrance off Salisbury Street.

7. The existing streetscape is an important element in order to ensure cohesion and integration which has been maintained. The relationship of the built form, with the surrounding area is integrated and complements both the residential and the commercial use patterns in the area.

The mature street trees have been retained at the south of the site. Extensive landscaping, more than what is currently at the site, has been proposed which will contribute to the localised environment.

The built form is setback significantly from this street, and the bulk of the form is orientated north to south, rather than running perpendicular with the street. This reduces any bulk from the street. Furthermore, the building is well set back with several level changes that will allow for visual interest and outdoor spaces rather than directly interfacing with built-up facades. The landscaping will create a pleasant environment and interest along the street.

Human scale is reflected with the levels of the development, where glazing / windows / materials change rather than being a flat or static surface. Green roofs will act to displace any height.

Visual interest is achieved with interesting colours and materials that reflect the youth vibe and the activated facility. The relatively small 'pocket' like areas and the series of separated courtyards will create variation. The green roof and the green house will be unique, visually interesting features.

The streetscape will be maintained with the proposal. The proposal is **consistent with the aims of the Policy**.



Movement / Flow / Circulation: Policy 14.15.33 (ii)

The general vehicle and pedestrian movement into the site from Gracefield Avenue and Salisbury Street, both pedestrian and vehicle-based movement, including the interaction between these two aspects.

8. Overall, carparking has a very minimal presence visually to the site i.e. it is not dominating or overpowering. There is only one small carpark situated off Salisbury Street, which does not detract from the main building entrance or pedestrian egress. Pedestrians and visitors have been considered as the forefront user group and this is reflected in the proposal, where there are several path links into the site.

The traffic assessment outlines safety and functionality of the spaces, and outlines the effects are acceptable / less than minor.

The carparks have screening in the form of landscaping that will buffer the carpark from the public street. Further strips of landscaping and trees along the residential boundaries to the north and east will also screen from these areas.

The vehicle circulation and the carparking has been considered for urban design inclusions. The proposal **is consistent with the aims of Policy.**

Residential Amenity: Policy 15.14.33a (iii) (A)

An exploration of surrounding local context; specifically, the Salvation Army building, residential blocks, other land uses, and the typical character of the area.

9. Given the activity will be increasing numbers of comings-and-goings, this will possibly affect existing amenity values relating to pleasantness / quietness. This is related to the nature of the development, where activity is increased. However much of the activity is inwardly focused, and contained within the mid-site, away from many residential boundaries.

There are already a number of non-residential uses in the vicinity. Future development and existing land use in wider vicinity that adapt the area from a purely residential character. Additionally, there are residential housing units and sheltered housing proposed on the site,

The adjacent blocks to the north and east are residential. The neighbours to the east of the site, on the corner of Gracefield Ave, are located immediately adjacent to café and event space and courtyards. Screening along the boundary is proposed and also courtyard spaces offset built forms from being immediately adjacent. The offset on the northern side (housing entry and security area) means living areas are divided from being immediately next to the existing and there is an offset from 9 Gracefield Ave.

The neighbours to the north off Gracefield Ave are located immediately adjacent to the housing and the main facility. The majority of the activity is focused in the mid-site, which is away from the northern boundaries. Internally, the building uses in the northern area will be relatively quiet uses, in the form of meeting and consultation rooms, which are in closest proximity to the northern residential interface.

The pattern of development is residential mixed with city fringe commercial to the west, east, and south. The development has a similar site coverage to existing buildings in the area. The development would not be out of context in terms of density, scale, city context



and character, given that there are a number of other higher density blocks, and buildings of a similar scale in the vicinity.

The main Youth Hub building has north south orientation with good outlook to the surrounding wider area. The residential housing blocks are located in orientation with the surrounding residential blocks. An acceptable level of sunlight and privacy has been provided for in the units and the layout reflects best use of available spaces, to ensure privacy and openness.

The landscape plan has a good division of public open space, semi-private resident shared spaces and private courtyard spaces. These areas provide for residents use at a communal level, or privately.

The proposal is considerate and reflective of the surrounding areas while contributing to inner city living future aims and the positive effects of a Youth Hub. The proposal **is consistent with the aims of Policy.**

CPTED: Policy 15.14.33a (iii) (A)

The safety of proposal in relation to both immediate and surrounding areas, and people traversing down Salisbury Street (and Gracefield Ave). The feeling of security and safety relating to aspects of public versus private space.

10. The activation of the site, including movement of people and general presence, will add to safety and security as there will be a constant passive surveillance to the site.

The pedestrian entrances are defined and have a clear hierarchy. The main entrance off Salisbury St has large open areas leading directly to the doors. Gated access points also dictate movement around the site and provide for safe and secure areas.

Lighting will be included at key locations. Appropriate vegetation and planting (below 1m where required) will be applied in consideration of providing clear sightlines. The planting in semi-public areas features low plantings with feature specimen trees, to ensure this.

An acceptable level of visibility, passive surveillance, strong pedestrian routes and a clear demarcation of private versus public space boundaries is evident, aligning with CPTED principles. The proposal **is consistent with the aims of the Policy.**

Activations / Frontages / Built Form: Policy 15.14.33a (iii) (B)

The form of the building including materiality and aesthetics, privacy and outlooks from site and immediately adjoining, use of ground floor at Salisbury café.

11. Colourations are proposed outside of the general surrounding colour scheme in area. However the incorporation of colours break up flat facades, and create visual appeal.

The additional height will change aspects regarding shading, recessions, and an increase in facing windows, upon surrounding areas. However, there is a good setback from boundary margins that will offset this.

The building layout and form generally avoids any large, flat or expansive walls. The varying ground floor layout, provides setbacks, openings, windows, courtyards and doors to



provide interest, rather than static flat surfaces. The upper floors are split using different sizes of blocks, varying material, modulation in facades and variations in the roof form, and the addition of the green roof.

The facade treatments create visual interest by using interesting materials, layers, battens and offsets where possible to create a variation in appearances. These also act to create human scale, as each built level is clearly defined. The street level from Salisbury Street is softened by planting and wall façade treatments at eye level to reduce any bulk. The building is setback a good distance, mitigating any sense of dominance from the street also.

Given the variation in form, the visual interest of the building design and the offset of landscaping and levels at a human scale, the built form and appearance is well considered. The proposal **is consistent with the aims of the Policy.**

Summary

12. The proposal is in keeping in character with the modified built-up landscape in which it is proposed. The Youth Hub can be suitably absorbed into the landscape fabric given the mix of residential and commercial activities existing, and the fringe nature of the area, meaning the development will blend into this particular urban environment.
13. The proposal aligns with the aims and objectives of the statutory district framework, specifically the Residential Design Guide 14.15.33, meeting the requirements. Amenity, function and integration matters; including access, streetscape, built forms, safety and CPTED, have been considered and are acceptable.
14. It is evident that urban design matters have been considered and developed over the duration of the proposal development. Efforts to ensure appropriate interfaces, material choices and offsets have been included to align with considered urban design matters. These will be further augmented by the landscaping proposed.
15. Landscape planting, including the design for separated courtyards, private spaces, recreational use, extensive planting and green roofs, is supported. The proposal also proposes a number of specimen trees and the retention of existing trees on site. These are considered appropriate, and furthermore of positive beneficial environmental gain (green roof and cultivation type areas). This will also assist in blending the proposal into the landscape, both at a localised and wider scale.
16. The development proposal will be complementary to the surrounding area, providing activation to the adapting urban environment, as well as the positive and beneficial aspects of a Youth Hub.

Appendix 9

Design Statement



TE HURIHANGA Ō RANGATAHI - THE YOUTH HUB

109 SALISBURY STREET

CHRISTCHURCH

DESIGN STATEMENT

14TH FEBRUARY 2020

INTRODUCTION

The Youth Hub is a unique project for Ōtautahi Christchurch, co-designed with many local youth and youth organisations. The aim is to create a youth-centric, attractive and accessible place that provides young people with a range of supports alongside employment, activities and housing opportunities. In this document we outline four key drivers that underpin the design of the project: 1) the aims, values and principles co-designed by local youth and organisations; 2) working sensitively and appropriately with the project site and context; 3) programming the site with the right mix of activities; and 4) resolving the Youth Hub's internal circulation in relation to the wider circulation patterns of its urban context.

1) AIMS AND VALUES

Through co-design workshops, local youth and youth organisations helped to establish architectural principles, aims and values for the Youth Hub. These are:

Whakapapa and Whenua – Acknowledge and work with the whakapapa of both the land and the people coming to the Youth Hub, and practice kaitiakitanga in taking care of all.

Accessible – A place of manaaki - welcoming and broadly available - socially, culturally and physically.

Empowerment – Mana-enhancing, hopeful and nurturing. A place that normalises creativity, exploration, education, development, improvement and excellence. Youth-centric creating a sense of belonging and ownership.

Inclusive - Open, celebrating of all diversity including gender. Culturally conscious and intentional where everyone is mutually respected.

Relationships - Treaty of Waitangi partnerships, community, trust, honesty, whakawhanaungatanga and participation

Safety - Mutual agreement, commitment, structure and conduct, collective security, sanctuary and protection and safe for all participants



APPLICATION

These values were carefully considered when making design decisions.

The Youth Hub is open and accessible. In all aspects of its production and ongoing operation it seeks to build partnerships – a process that has begun with the co-design process itself. Many voices have been engaged in the design development and there are on-going opportunities to work with craftspeople in order to bring layers of narrative into subsequent stages of the project. The design proposal has a strong sense of entry to the site, with reference to Māori and Pacific architecture in the entrances, along with a sense of procession and readiness for pōwhiri. Sense of place is strongly reinforced through the landscape design, including a diverse range of courtyard spaces, terraces and native plantings. The very purpose of the hub is to provide young people with support to enhance themselves and their mana. These are all ways the design has sought to engage with Whakapapa and Whenua.

The design celebrates a sense of discovery and diversity. It is designed and experienced not as a homogenous singular building, but as a complex of many spaces, forms and volumes, each with different purpose and sense of place, but forming one coherent place. This results in a building that is fundamentally diverse, inclusive and accessible, with layering that provides areas of protective sanctuary alongside areas for social interaction and gathering.

The greenhouses on the rooftop provide a visual presence of the growth and well-being associated with the Cultivate market gardens. They give interest and purposeful idiosyncrasy to the design, indicating this is not an ordinary place. Along with the greenhouses, the bright colours of the Youth Hub reflect the desires of young people, who asked for colour and for the architecture to be unique and inspirational. Upon seeing the design a project stakeholder described it as “the hope machine”.

2) LOCATION AND CONTEXT

The Youth Hub is located within the central city to ensure it is accessible for young people and well connected to inner city amenities and facilities. Previously a bowling club, the large site affords great opportunities for the supported housing core of the Youth Hub to be in a residential context, whilst bridging the Youth Hub to the mixed use and central city amenities.

Comprehensive contextual analysis was undertaken to inform the design. The immediate vicinity is fairly green and with street trees. Surrounding buildings are predominantly one and two-level of a reasonably fine grain and largely with pitched roofs. Salisbury street is a busy one-way road which borders between mixed-use and residential. Gracefield Avenue is a quiet and charming inner-city residential street.

APPLICATION

The dual street frontages of the site, with different character and presence, afford the opportunity for a more public face to the busier, city-oriented Salisbury Street and a quieter, more residential character to the site frontage onto Gracefield Avenue.

The main Salisbury Street frontage has a prominent entry house and a small café building. These smallish single-storied buildings have pitched roofs and fit comfortably into the character of the immediate context. The yellow roof of the entry house and its entry canopy mark it as something unique and clearly define the main site entrance. Between these buildings is a retained specimen maple tree and to the west is a parking and drop-off area.

The colourful main building of the complex is set back in the centre of the site, with greenhouses on top, which vary and repeat the pitched roof language. The greenhouses help to unify the project and give a sense of its scale, with visibility from various places. However, to nestle into the context there is never an overall view of the main building, but rather a series of views obscured by plantings and other building forms, with the glasshouses almost floating above.

The Gracefield Avenue frontage has predominantly three-storey row house apartments with strong residential character. These have alternating pitched roofs, a fine grain and their domestic nature, soft materials (brick and timber) and front courtyard gardens with trees make good contribution to the street. Kitchens, entrances and circulation spaces face onto the street providing a sense of life and security. A canopy over a secondary gated site entry repeats the language of the Salisbury Street entry house and a two-storey onsite managers unit occupies the North-East corner of the site.



The internal boundaries are predominantly planted. Along the western edge is a two-storied residential building of fine grain and residential character, with alternating pitched roofs and façade differentiation. Between this building and the boundary are individual courtyard gardens and trees. Elsewhere are planted courtyards or car parking of permeable pavers, with substantial planting especially where the main building comes close to boundaries. Planting throughout the site, including in gardens and courtyards, and on terraces and green roofs, draws on and adds to the green character of the context.

3) FUNCTIONS AND PROGRAMME OF USES

The functions and activities in the Youth Hub form a varied programme of uses with complex interactions. These functions involve and represent a range of stakeholders and users, with varied spatial and functional requirements. There are quite different public and private and staff and youth-centric requirements for different uses within the programme.

APPLICATION

Sheltered housing is in the most private area of the site, allowing for the required sanctuary and retreat. More conventional housing, to graduate to, is located along Gracefield Avenue. The most public aspects of the programme, a cafe and entry house are located on the busier Salisbury Street. The café provides skills training for young people and an opportunity for engagement with a broader public. The internal boundaries all have quiet and private uses backing onto them.

A range of recreation and activities appealing to young people are located in the heart of the site. These include arts, gallery and creative spaces, performance and events, sports areas, as well as informal and multi-use areas. Landscaped outdoor areas, courtyards and roof terraces complement and add to these functions.

There are many support services for young people within the Youth Hub. These includes mental and physical health, advocacy and support services, education and vocational training, social workers, employment and legal advice, as well as spaces for government agencies. Located in the centre north of the complex, the support services are designed to be proximate and easily accessible but not overbearingly present for those visiting or living at the Youth Hub. For the many youth organisations that will be based here, the Youth Hub provides access to young people alongside high quality work spaces shared with similar organisations.

4) CONNECTIONS AND CIRCULATION

The programme of uses and site require clarity and layering of public and private spaces for legibility, way-finding and functionality. Furthermore, the various functions and users (primarily young people and the staff of support organisations) require careful arrangement in order to avoid conflict. Some of the uses in the programme need to be shared, others adjacent, others discrete, and others strictly separated.

APPLICATION

The proposal includes two highly legible primary entries. The entry house fronting onto Salisbury Street provides a clear and welcoming presence to the most public entry of the site, providing access to all the shared areas of the Youth Hub. Reminiscent of the entry house is a covered entry on Gracefield Avenue which allows a secondary access for more private uses, including the sheltered housing and the support services. The apartments fronting onto Gracefield Avenue all have their own front entry directly off the street, with a threshold of outdoor living and planting.

The overall organisation of the Youth Hub is that of a complex or campus. The various functions are connected and separated through shared, semi-private or private circulation spaces including atria, galleries, terraces and courtyards. Variations in character of space, such as narrow and wide, tight and voluminous, and inside or out, add layering and richness to the experience of moving through the Youth Hub. They also add thresholds between uses and help manage interactions.



Playful aspects of circulation such as outdoor and atrium stairs help create three-dimensional connections, linking together the multiple levels and uses. Service access for the greenhouses (and curiosity/activity to the public facade) is provided by a fixed scissor-lift in the drop-off area off Salisbury Street.

For safety, the exposed circulation adds to the feeling of passive surveillance throughout the site and the plan avoids entrapment zones or hidden spaces in the common areas, with most areas having at least two access ways. Multiple access ways (and reasons to go to any particular area), help to avoid stigma and socially awkward situations, as does the option of a more discrete entry to the more private uses (i.e. health appointments via Gracefield Avenue).

Moving through the entry house and past the low gallery and quiet courtyard space brings visitors into the voluminous heart of the hub, where activities and outdoor areas open out. From here it is easy to head to training rooms or the atrium from which the support services, the outdoor terraces or Cultivate greenhouses can be accessed.

Entry to the sheltered housing is through the north of the shared atrium in the support services area into a communal courtyard and living space discrete and private from the other programmes of the complex. The spaces layered the deepest and most private in the complex are the bedrooms of the sheltered housing or their ground floor private courtyards.

SUMMARY

The Youth Hub design works to enmesh these four drivers to cater to wide range of stakeholders but most importantly, young people with diverse realities. Having worked to include these diverse voices in the development of the principles that sit behind the design we feel confident that the resultant Hub will be a special one for local youth. In addition, the design works to carefully compliment the character and function of the surrounding neighbourhood with a more active frontage to Salisbury Street and a more residential character to Gracefield Ave. This project is a significant investment in the future of Ōtautahi, socially, culturally and architecturally and we hope it will foster a sense of belonging and pride for Ōtautahi youth and the wider community.

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Appendix 10

Acoustic Assessment



NOVO group
Planning. Traffic. Development.

Environment Noise Assessment

Prepared for

**TE HURIHANGA Ō
RANGATAHI – THE
YOUTH HUB**

**109 Salisbury Street
Christchurch**

February 2020



Environmental Noise Assessment

Prepared for

Te Hurihanga Ō Rangatahi – The Youth Hub

109 Salisbury Street
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Introduction

1. Te Hurihanga Ō Rangatahi – The Youth Hub has commissioned Novo Group Limited (Novo Group) to prepare an Acoustic Assessment for the development of a youth focussed community facility at 109 Salisbury Street in central Christchurch.
2. This report considers the existing / ambient noise environment, the noise characteristics predicted in association with the proposed activity, and the relevant provisions in the District Plan. Where noise levels are found to exceed the applicable standards in the District Plan, potential noise effects are assessed and conclusions are provided as to the potential for other persons to be adversely affected by noise from the proposed activity, the significance and acceptability of any noise effects, and the need for any conditions or other measures to adequately avoid, remedy or mitigate noise effects.

The Proposal

3. It is proposed to develop the site at 109 Salisbury Street to provide for a range of activities including residential, entertainment, health care office, food and beverage plus an urban farm. The activity will be supported by ten car parking spaces (including two mobility spaces) in two parking areas, with access to Salisbury Street and Gracefield Avenue. The site location is illustrated in **Figure 1**.



Figure 1: Site Location



4. The particulars of the relevant noise sources associated with the activities are:
 - i. Building break-out noise during events;
 - ii. Mechanical plant noise;
 - iii. Outdoor recreational area, specifically the basketball court;
 - iv. Café;
 - v. Car parks

District Plan

5. The site and surrounds are within the Central City (Category 3 Precinct) with the project required to comply with Christchurch District Plan Rule 6.1.5.2.2 'Noise limits in the Central City'.
6. A detailed assessment of relevant rules applying to the proposal is set out in the AEE, and this concludes that resource consent is required for a non-complying activity.
7. Of relevance to this acoustic assessment, the following noise performance standards under Rule 6.1.5.2.2 of the Christchurch District Plan apply (refer to Table 2 below). The rules require that noise be assessed in accordance with NZS 6802:2008 - Acoustics Environmental Noise.

Table 1: Zone Noise Limit

Category 3 Precinct	Time (hrs)	Noise Limit (dB)	
		LAEq	LAmx
All activities	07:00-23:00	55	85
All activities	23:00-07:00	45	75

8. However, the construction activities (enabling works) during the construction phase would according to the Christchurch District Plan need to comply with NZS 6803:1999 – Construction Noise. Under Rule 6.1.6.1.1 Activity Standard P2, all construction is required to be managed in general accordance with NZS 6803:1999 Acoustics- Construction Noise, within prescribed noise limits according to the duration of the work.

The Existing Noise Environment

9. A site visit was conducted on 15th and 16th January 2019 to make in situ observations and supplement it with ambient noise measurements. During the site visit, three ambient noise measurements were taken during the daytime (12:00-13:00) and evening time (21:30 – 22:00). For the location of the measurements refer to Figure 2 to Figure 5 below. The sound level meter was set up at a height of between 1.2 and 1.5 metres above ground level and 3.5metres away from reflecting surfaces. The weather conditions during the measurements were partly cloudy, gentle breeze and temperature at 18°C.
10. A Brüel & Kjær 2238 Mediator Sound Level Meter (Type 1) was used for the measurements and carried out in accordance with NZS 6801:2008 “Acoustics – Measurement of environmental sound” and NZS



6802:2008 “Acoustics - Environmental noise”. The sound level meter was calibrated in situ before and after the measurement within the required tolerances. The sound level meter and calibrator are within current Laboratory Calibration periods (calibration certificates available upon request).



Figure 2: Measurement locations



Figure 3: Noise measurement N1



Figure 4: Noise measurement N2



Figure 5: Noise measurement N3

11. The acoustic environment was observed as being typical of an urban environment near a low speed distributor road. The main audible noise source at the time of measurements was the road traffic noise on Salisbury Street. The site itself was particularly ‘quiet’ during the site visit with low ambient levels due to infrequent vehicle movement on the surrounding roads at the time of assessment. According to the measurement results the noise levels ranged from 49dB to 53dB during the afternoon and from 40dB to 42dB during the evening (refer to Table 2 below).

Table 2: Noise Measurement Results

Location	Time	dB LAeq	dB LAFmax
N1	Day	49	70
	Evening	42	54
N2	Day	52	76
	Evening	40	53
N3	Day	53	68
	Evening	41	60

12. Using the Annual Average Daily Traffic (AADT) figures for the day and night time periods, the existing background noise level at the residential boundaries fronting Salisbury Street is conservatively estimated, using the CRTN method, at 63dB LAeq(15min) for the daytime (07:00 – 23:00) and 53dB LAeq(15min) for the night time (23:00 – 07:00).
13. The traffic figure specifically between the hours of 22:00 – 23:00 has also been used to estimate the noise level to be compared with the estimated noise level from the Salisbury Car Park to assess the potential effects during this period with respect to vehicle exit movements after events. The existing



background noise level at the residential boundaries fronting Salisbury Street during between the hours of 22:00 – 23:00 is estimated at 58dB LAeq(15min).

Assumptions

14. The following fundamental assumptions are included as part of the predicted noise levels from the proposal's activities, noting that in accordance with NZS6802:2008, the worst case 15min LAeq noise levels for each activity is calculated and compared to the relevant limits:
 - i. Two metre acoustic fence will be installed between all neighbouring properties;
 - ii. Event functions are assumed to take place during daytime hours only;
 - iii. It is assumed that an events management plan will be implemented with each event and includes crowd control measures mitigating people congregating in courtyards during events as well as in the Salisbury Street Car Park after events (ensuring people leave promptly);
 - iv. Basketball court to be used during daytime hours only;
 - v. Café operational hours to run from 07:00 – 17:00 and no alcohol provided;
 - vi. Gracefield Avenue Car Park to be used predominantly during the daytime hours (07:00 – 23:00), with very low trip generation during the morning peak (05:00 – 07:00) which falls within the night time period;
 - vii. Salisbury Street Car Park to be used during the daytime period only (07:00 – 23:00); and
 - viii. No music played outside the café as part of its operations, if music is played inside its played at background level only.

Noise Level Predictions

Entertainment/Functions

15. The concept designs include an events room where amplified music will be played during events. The proposed construction will be an exterior precast concrete wall with no openings along the southern and eastern elevation of the events room as well as precast concrete roof with more building levels above.
16. The noise break-out has been estimated using the methodology of British Standard BS EN 12354-4:2000 '*Building acoustics. Estimation of acoustic performance in buildings from the performance of elements. Transmission of indoor sound to the outside*'.
17. The sound reduction is dependent on the thickness of the exterior concrete walls. Using a conservative thickness of 100mm, with a sound reduction rating (Rw+Ctr) of 41dB, the noise level at the nearest residential boundary is calculated at a level of **65dB LAeq**, using the following estimates and assumptions:
 - i. An assumed noise level within the events room of 100dB during events;
 - ii. The exterior noise level 1m from the façade is 76dB in accordance with BS EN 12354-4:2000;



- iii. Distance to neighbouring boundary is 3.6m (9 Gracefield Avenue, Christchurch), with a noise reduction of 11dB in accordance with the distance law; and
- 18. It is estimated that the indoor music from events will exceed the daytime limit at the nearest boundary if the concrete walls are constructed at 100mm.
- 19. However, constructing the concrete walls at a minimum of 150mm, using the calculation methodology as above, will give an additional noise reduction of 11dB putting the estimated boundary level at 54dB LAeq.
- 20. Compliance will therefore be achieved if concrete walls are a minimum thickness of 150mm.

Mechanical Plant Noise

- 21. The associated heating, ventilation and cooling (HVAC) as well as refrigeration systems have not been specified yet, however these services can be selected and designed with standard screening measures and made to comply

Outdoor Recreational Area (Basketball Court)

- 22. Existing literature relating to recorded noise levels at the edge of a basketball game, was sourced and used, which included all types of noise related to a typical game (dribbling, running, shouting).

Table 3: Indoor Basketball Noise Levels

Descriptor	dBA	Octave Band Centre Frequencies - Hz							
		63	125	250	500	1k	2k	4k	8k
L _{eq} 15min	73	73	77	71	69	70	65	58	48

- 23. The basketball court is shielded by the two story youth housing building along the western elevation of the recreation area, the nearest residential boundary to the south (103 Salisbury Street) is 14m away. The predicted noise level from the basketball court at this boundary is 50dBA, with a reasonable assumption that the durational use of the court will be less than 80% of the entire daytime period.
- 24. It is therefore estimated that the outdoor recreational area will comply with the daytime noise limit at all surrounding properties.

Car Park Noise

- 25. The car parking supply proposed at the site is ten spaces, these comprise:
 - i. Four visitor spaces (accessed via Salisbury Street);
 - ii. Three resident spaces (accessed via Gracefield Avenue); and
 - iii. Three staff spaces (accessed via Gracefield Avenue).
- 26. As per the Novo Group Integrated Transport Assessment (ITA), the six car parks located off Gracefield Avenue are anticipated to generate approximately three vehicle movements per peak hour and the four



car parks located off Salisbury Street are anticipated to generate approximately 11 vehicle movements per peak hour. An additional assumption in the ITA is that there could be a 44 vehicle movement turnover per peak hour associated with events for the car park located off Salisbury Street.

27. The following conservative vehicle movement per 15minute peak has, therefore, been applied to the noise prediction calculations:
 - i. Two vehicle movements for the car park located off Gracefield Avenue for the daytime period and one vehicle movement for the night time period (05:00 – 07:00);
 - ii. 6 vehicle movements for the car park located off Salisbury Street for normal day to day activities during the daytime period; and
 - iii. 22 vehicle movements for the car park located off Salisbury Street during events (entering or exiting) during the daytime period.
28. The detailed parameters used in the estimation of the Gracefield Avenue Car Park noise levels are as follows:
 - i. Sound exposure level (LAE) of 74dB at 5m for vehicle movement;
 - ii. a typical L_{Amax} level of 66dB at 10m for car doors closing;
 - iii. A distance of 1.5m to the boundary of 27 Gracefield Avenue in terms of vehicle movement and car door slams,
 - iv. A -5dB duration correction in terms of the LA_{eq} value in accordance with NZS6802:2008 for the daytime period;
 - v. A -5dB shielding correction in terms of the LA_{eq} and L_{max} values with assumed acoustic fence in place;
 - vi. Assuming there is no use of the car park after 23:00 and a worst case of one vehicle per 15min peak between the hours of 05:00 – 07:00 for assessing the night time effect.
29. The resultant daytime noise level at the northern boundary (27 Gracefield Avenue) from the Gracefield Avenue Car Park is 48dB LA_{eq} and 77dB L_{max}. It is estimated that the Gracefield Avenue Car Park will therefore comply with the daytime noise limit of 55dB LA_{eq} as well as 85dB L_{max} at all neighbouring properties.
30. The resultant night time noise level at the northern boundary (27 Gracefield Avenue) is 50dB LA_{eq} and 78dB L_{max}. The estimated noise levels will therefore exceed the night time noise limits of 45dB LA_{eq} as well as 75dB L_{max} if the car park spaces nearest to the boundary is used during the night time hours.
31. However, if the parking spaces on the southern portion of the car park are designated for staff only, with the car movement approximately 7m from the boundary, the resultant night time noise level at the boundary would be 41dB LA_{eq} and 69dB L_{max}, which will comply with the night time noise limits.
32. The detailed parameters used in the estimation of the Salisbury Street Car Park noise levels are as follows:
 - i. Sound exposure level (LAE) of 74dB at 5m for vehicle movement;



- ii. a typical L_{max} level of 66dB at 10m for car doors closing;
 - iii. A distance of 0.8m to the boundary of 103 Salisbury Street in terms of vehicle movement but a distance of 5m for car door slams,
 - iv. A -5dB duration correction in terms of the L_{Aeq} value in accordance with NZS6802:2008 for the daytime period;
 - v. A -5dB shielding correction in terms of the L_{Aeq} and L_{max} values with assumed acoustic fence in place;
 - vi. Assuming no use of the car park during the night time period (23:00 - 7:00).
33. The resultant daytime noise level at the western boundary (103 Salisbury Street), in terms of normal day to day activities, is 59dB L_{Aeq} and 62dB L_{max}. It is estimated that the Salisbury Street Car Park will exceed the daytime noise limit of 55dB L_{Aeq} but comply with the 85dB L_{max} at the nearest neighbouring property.
34. However, the noise effect from the day-to-day activities is unlikely to be considered unreasonable when comparing the estimated car park noise levels (59dB) with the estimated existing daytime background level (63dB), causing an increase of 1.5dB, which is a barely perceptible change.
35. The resultant daytime noise level at the western boundary (103 Salisbury Street), during events, is 63dB L_{Aeq} and 62dB L_{max}. It is estimated that, during events, the Salisbury Street Car Park will also exceed the daytime noise limit of 55dB L_{Aeq} but comply with the 85dB L_{max} at the nearest neighbouring property. When comparing the estimated car park noise levels (63dB) with the estimated 22:00 – 23:00 existing background level (58dB), causing an increase of 6dB, which is a noticeable change.
36. However, the noise is unlikely to be considered unreasonable and adversely affect any person due to the relatively infrequent occasions events will be held as well as the receiver at 103 Salisbury Street being sufficiently sound insulated to protect themselves from surrounding land uses (after review of the building consent plans, noting e.g. brick veneer and double glazing).

Café

37. The main noise source associated with the café is the patrons, especially patrons using the café courtyard. The café courtyard will likely contain small groups and with no alcohol served it is unlikely to be rowdy.
38. In terms of the area of the courtyard it is assumed that 10 seated patrons can occupy the area, of which 50% are talking simultaneously at a sound power level of 70dBA per person.
39. Other parameters used in the estimation of the Cafe noise levels are as follows:
- i. An assumed setback of 4m from the eastern boundary; and
 - ii. A duration correction of -2dB, which presents a scenario where the courtyard will be occupied for the entire operational period (07:00 -17:00).
40. The resultant daytime noise level from the café at the eastern boundary (3-9 Salisbury Street) is 55dB L_{Aeq}. It is therefore estimated that the café will comply with the daytime noise limit at the nearest boundary.



Noise Effects Assessment

41. The proposed Youth Hub contains a number of intermittent noise sources associated with people congregating and playing. Predicted sound levels are generally between 50-55 dB LAeq(15min). The majority of activities will occur during daytime hours (0700-1700h). The sound from people playing and talking is compatible with a central city environment.
42. Music noise from evening functions (1700-2200h) could likely be audible over the evening/night time ambient sound, however, with the level estimated at 50dB (assuming 150mm concrete wall) it is within compliance and acceptable.
43. It is estimated that for most of the components of the proposal, compliance will be achieved with the Residential Central City Zone noise limits.
44. Even though the Salisbury Car Park is expected to exceed the daytime LAeq noise limit, the noise effect from the day-to-day activities is unlikely to be considered unreasonable due to the barely perceptible change in respect to the existing background level.
45. As for the noise effect during vehicle exits after events, causing a noticeable 6dB increase with respect to the existing background level between the hours of 22:00 – 23:00, it is not considered unreasonable or will not adversely affect any person due to the relatively infrequent occasions events will be held as well as the receiver at 103 Salisbury Street being sufficiently sound insulated to protect themselves from surrounding land uses (after review of the building consent plans, noting e.g. brick veneer and double glazing).

Recommendations

46. The following is recommended to achieve the permitted activity standards, or as good practice and to minimise any effects on neighbouring properties:
 - i. The construction activities will be managed and controlled in accordance with NZS6803:1999;
 - ii. The acoustic fence should be installed at a height of 2m and shall be maintained in good condition with no gaps and shall have a minimum surface mass of 8.0kg/m².
 - iii. Concrete walls of events room to be a minimum of 150mm thick;
 - iv. Implementing event management plans to make sure people do not congregate in courtyards during events or in the car parks after events (ensuring people leave promptly from the car parks);
 - v. Standard screening for HVAC systems;
 - vi. Maintain a 4m setback from the eastern boundary with regards to the setup of the café courtyard seating area; and
 - vii. ensuring that staff arriving in the early morning (05:00 – 07:00) can only park in the three southern parking spaces of the Gracefield Avenue Car Park.



Conclusion

47. Novo Group has been commissioned to prepare an acoustic assessment for a youth focussed community facility at 109 Salisbury Street in central Christchurch.
48. The assessment concludes that, if the above recommendations are implemented, the proposal will have an acceptable effect and no persons will be adversely affected.

Appendix 11

**Further Information Response
25.05.2020**

Te Hurihanga Ō Rangatahi – The Youth Hub

RFI Responses

22nd May 2020

Please see responses to queries written in-line in blue below:

Previous Question

I note that the current layout for the car parking in the Gracefield Avenue car parking area would require drivers to undertake a number of manoeuvres to get into the spaces, due in part to the narrow entrance. Can the applicant please comment on the option of widening the access gates on the southern side to provide better manoeuvring and may also provide the opportunity for better visibility for drivers to the north.

We have reconsidered the Gracefield Avenue entry in light of this and other comments and provide an updated design in the documents attached (most clearly indicated in A200 and A300). The design has a single 1200mm-wide pedestrian/bicycle path on the southern side of the entry and a widened vehicle access to the north of this. The fence to the on-site manager's unit has also been lowered to increase visibility (refer to A300). See also screenshot below showing a view from Gracefield Avenue.



Points to clarify

1. What opportunities are there for passive surveillance of shared outdoor spaces within the site from surrounding onsite habitable spaces? There appears to be minimal passive surveillance from the surrounding onsite building edges over the Northern Carpark area. Passive surveillance of this entrance is of interest because it is the less public and more discrete route to the assisted housing units as well as to the Health Centre.

The interior facades of the site are yet to be fully developed. However, the design has been reconsidered relating to this question and further glazing and information added. Specifically, windows have been added to the onsite manager unit - near the kitchen facing the entry passage and at the stairs on the rear, facing the carpark area (refer screenshot below). The reduced fence height to the onsite manager unit also improves passive surveillance to this area (see screenshot above). Also further articulation has been added to the stairway of the northern apartment, with a bay window seat and desk occupying the stair landings – improving use of these spaces and passive surveillance of the carpark area. The Gracefield avenue façade of this northern unit has also been addressed in the programme with a bedroom facing the street on the ground level, living room with bay window at first level (as well as a bench/leaner in the stair landing) and desk/seating on upper stair landing (including window added to the north). The entry to main building from the carpark has been made substantially more transparent and direct and the glazing on the floor above has been increased. All of these aspects maintain the atmosphere of a more discrete entrance, but increase opportunities for passive surveillance. Refer to plans, elevations and screenshots below:



Onsite Manager Unit carpark windows



Northern Apartment Gracefield Avenue Facade



Northern Apartment Stairway Section – showing programme and carpark windows



Entry to main building from northern carpark.

2. Regarding the Gracefield Avenue housing:

- Are the doors to the five Gracefield Avenue terraces glazed?

Yes, the entry doors are glazed – please refer A300 and screenshot below.



Gracefield avenue glazed entry doors

- Please update the Ground Floor and Second Floor Plans to reflect the window locations as shown on the Eastern Elevation (so that the elevations and floor plans are consistent, as not all the windows match).

Plans have been updated to correct this (bay windows were incorrectly visible on some plans).

3. Is there a door between the kitchen of the Housing Entry and the corridor to the Assisted Housing?

As noted above, the internals of the project require substantially more design development, which would be undertaken contingent upon receiving a resource consent. However, relating to this point a door has been added in this location, as well as a door direct from the shared courtyard to the corridor. See A200.

4. What signage is proposed (if any) on the Salisbury Street and Gracefield Avenue frontages?

Signage is yet to be designed however will comply with district plan requirements or a separate resource consent will be sought.

5. Where are waste bins located? Location of the café and courtyard bins should be considered for their proximity to the adjacent residents at 3-6 Gracefield Avenue.

Refer to landscape plans. An additional bin store has been added into the ground floor of the northern apartment facing the northern carpark (refer to A200).

6. Please provide an image or link to the 'Kowhai Glo' Colorsteel cladding.



Photograph of a built example of a 'Kowhai Glo' Colorsteel roof.

<https://www.eboss.co.nz/assets/Uploads/ebossnow/2014/02/KowhaiGlow.jpg>

7. Regarding the western elevation of the Assisted Housing, the first floor window sill height appears to be quite high above the floor level, and young people may not be tall enough to look out of these higher silled windows. Can you confirm the sill heights above the floor level and explain the rationale behind the sill heights if they are relatively high?

FFL information has been added to A301. From the outside the cladding comes down below the concrete slab. Opening sash height is set to 1140mm above FFL, in accordance with NZBC F4 (minimum 1100mm). The non-opening windows are approximately 650mm above first floor FFL. Refer also to section screenshot below.



8. Regarding cycle parking, is there a separator between the cycle parking and staff car parking? *Appendix 7.5.2* requires that cycle parking facilities be located so that the bicycle is at no risk of damage from **vehicle movements** within the **site**.

Yes – it is proposed to have posts (hopefully timber, or otherwise steel) at each vertical bicycle stand location, which support bicycle stands, provide separator as described in question and support the roof/terrace above. Refer to screenshot below:



9. Some of the cycle parking spaces shown on the plans are short of the 1.8m depth required in the Plan (e.g. 1.450m and 1.1m) – will these be workable as cycle parking spaces? Are they all wall mounted cycle parks?

Wall mounted stands are proposed internally. The external cycle parking was identified previously as complying with the District Plan dimensions. The dimensions of the cycle parking were discussed around para 33 of the ITA, which noted the internal cycle parking is wall mounted. The external cycle parks that dimension at 1.45m (Figure 8 of the ITA) comply with Diagram 2 of the District Plan cycle parking layout standard (Double sided – front wheels overlapped).

10. The internal corridor width between the central stairwell (from the Arts Centre) and the Training Room appears to be quite narrow in terms of visibility and accessibility (2m). Is this intentionally narrow to act as an informal security line or transition between the “hang out area” to the south and the Health Precinct (including consultation rooms and access to the more secure residential rooms for young people)?

As noted above, the internals of the project require substantially more design development, which would be undertaken contingent upon receiving a resource consent. With regard to this question in particular, there is a deliberate transition zone in this area between the arts/recreation space to the south and the other uses to the north. The atrium is also part of this transition. These aspects are all intended to be designed further in subsequent design stages and in collaboration with young people and the project partnering and stakeholder organisations.

Is it possible to provide detail of plant species on the landscape plan in the areas that will be interfaces with other sites and road boundaries?

Refer to landscape plans.

Urban Design Initial comments / recommendations

1. The entrance into the Health Centre (from Gracefield Avenue through the staff carpark) is obscured. Consider the accessibility and experience of the route between Gracefield Avenue and the entrances to the Health Centre reception, as well as the Assisted Housing entry. Could there be clearer sightlines to doors and the main reception area? Could there be appropriate paving (in lieu of permeable paving with groundcover) and articulation of a pedestrian pathway across the northern carpark, between the closest pedestrian gate and the main door?

As per previous note, the internal aspects of the project need further work. However, we have reconsidered this entrance and adjusted as per the attached plans (A200), landscape plans and the screenshots below. Please note these views both show the delineation of the accessway and the specific materials shown (brick as asphalt) are not representative of the actual selections. – Refer to landscape plans for these. This includes introducing a single and widened pedestrian path on the south side of the entry. This is articulated with a flat portion of roof and also with articulation in the ground plane with paving. This also aligns with a revised glazed entry with doors facing direct to the east (rather the previous side entry). Furthermore, and as indicated in the first screenshot above, the proposed gate is intended to be a slatted gate (ideally timber) that has visibility through even when closed.



Entry View from Gracefield Avenue



Entry view of main building across carpark from within entry canopy.

2. Could there also be clearer sightlines to the covered cycle parking area within the northern carpark?

The articulation of paving as per point above, along with the further information in the support structure/separation help to make the cycle parking more legible in the northern carpark. See screenshot below. Again, note the indication is of delineation rather than specific materials.



3. Regarding Windows

1. For the Gracefield Avenue housing, could you consider including a window looking out from the ground level Study of the northernmost of the five units, and from the kitchen in the southernmost unit to improve opportunities for passive surveillance from habitable rooms?

The Gracefield façade and programme/planning arrangement have been adjusted to improve opportunities for passive surveillance and activity/relationship with the street. The northern unit has been rearranged as indicated in response to point 1 and associated screenshots above. The southern (accessible) unit has been rearranged internally so the accessible bedroom has a window looking out onto Gracefield avenue and the rearranged planning means the existing window to the kitchen has better activity, visibility and connection (previously being a corridor). The study area on the first floor has had a bay window added to increase interaction with the street. While these adjustments have mostly been addressed in the southern and northern units (with some adjustments to other units to fit – ie. location of bay windows), some aspects of the increased interaction with the street are expected to be developed throughout in subsequent design stages – ie the occupied stairway landings. Please refer to A200, A201, A202 and A300, as well as screen shot below.



Southern Apartment Gracefield Avenue Façade.

2. Consider windows for passive surveillance on the northern face of the Gracefield Avenue Housing (5 units) and the southern side of the caretakers unit.

As indicated in response to point 1 above and associated screenshot, one window has been added to each of these walls. These are not large (to avoid too much sense of surveillance and corresponding exposure of occupants) and also as these will need to be fire rated walls for safe egress.

3. Consider whether the large bathroom window on the north side of the caretakers unit is a potential privacy issue and may be more useful for passive surveillance elsewhere.

This window has been made smaller to allow more privacy to the bathroom. Sill height is 1400mm. A taller window has been added to the eastern end of the north façade. Refer to A201 and A301 and screenshot below.



Just one further question related to matter 14.15.33.a.3.A (incorporation of CPTED), can you provide more detail of which gates within the site are to be locked and at what times? The potential concern here is the safety of the sheltered housing area. The AEE has stated that access to the sheltered housing is through the Entry House and through the northern atrium (via the Housing reception), however there is also a potential access route along the western boundary (off Salisbury Street), and through two gates leading directly to the corridor which accesses bedrooms. Clarification of whether these gates are locked, and if so at what times, would assist in addressing Rule 14.15.33.a.3.A.

These gates, lanes and doors are for fire escape purposes and/or service access only and will be monitored and locked accordingly. Where for fire egress only they will be one-way doors with push-bar exits and where for service access they will be locked and controlled to ensure only service access. They are in no way intended for everyday or general use.